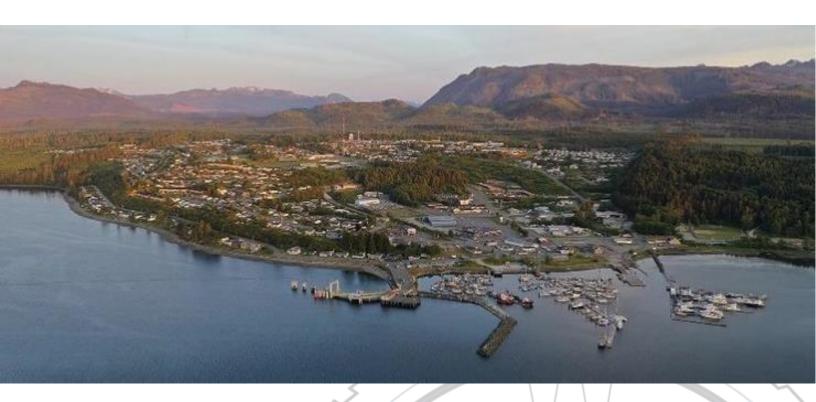
Town of Port McNeill



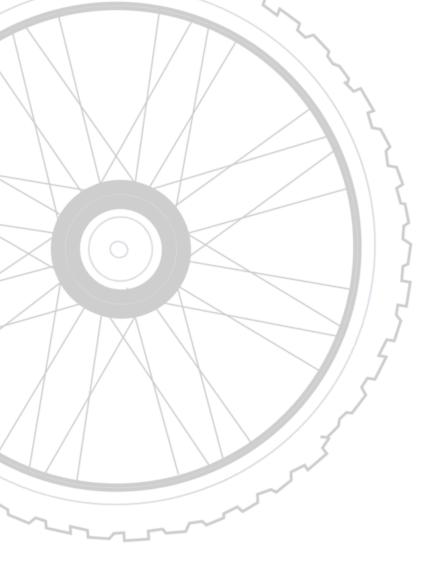
Port McNeill BC



Active Transportation Plan

B.C. Active Transportation Infrastructure Grants Program

Project #065OA117031





March 26, 2021

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Executive Summary

The Town of Port McNeill is a cozy ocean side community on the north east side of Vancouver Island facing the coastal mountains of British Columbia's mainland. Nature is at the doorstep with outdoor activities an integral part of life. Port McNeill is planning for the future, for people within the community and for the future families. The Active Transportation Plan (ATP) will help to guide the Town's investment in active transportation over the next 30 years.

The future of active transportation within the Town of Port McNeill was envisioned by the community in a process that began in August 2020. This planning process has occurred jointly with the Town's Official Community Plan (OCP) and Zoning Bylaw review. In the public engagement process the community identified the priorities in active transportation for the Town. The process included community workshops with stakeholders, youth, and seniors, on-line surveys, and an interactive map and workbook exercise. The community established that the ATP was to provide accessible connectors that will encourage sustainable transportation options in the Town.

VISION

During the OCP engagement the vision for the community was described by the community.

Access to the natural environment, rural lifestyle, industrial heritage, and family are identified by residents as key community values. The vision statement is an outline of what the community wishes to become. It, when combined with the guiding principles and goals, provide a guide to help in decisions making.

Port McNeill will continue to provide an atmosphere of well-being. A community that encourages healthy living and is a hometown of choice for families, entrepreneurs, and innovative industry, where people wish to grow and retire. The Town provides an environment and community where people can succeed. This is nurtured through the rural lifestyle, access to a nature and a sustainable environment. It is a place that fosters individual potential and embraces new possibilities. Here we can pursue our dreams for generations to come.



GUIDING PRINCIPLES

Implementing community values requires a focus on the *economic*, *social*, *cultural*, *and environmental* aspects of the community. These four aspects directly impact the health and happiness of all the residents in Port McNeill. Community well-being refers to the optimal quality of healthy community life, which is the inherent goal of the community plan. Community well-being addresses the pillars of sustainable development while expanding the pillars to include the key community values. These values were identified during the consultation and engagement process.



Figure 1: Community Well-being

GOALS identified in the OCP engagement process that directly relate to the ATP include economic prosperity, social equity, cultural vitality, and environmental sustainability. Each of these goals is supported by more specific objectives and targets that elaborate on how each goal can contribute to a successful active transportation network and how the network will benefit the community.

Encourage tourism-based industries

Provide accessible transportation systems

Foster a healthy community

Enable access to the natural environment

Reduction of reliance on vehicles

Transportation options to reduce Green House Gas (GHG) emissions

The Town of Port McNeill will also meet the goals of the Province of B.C. Active Transportation Network Grant Program that include:

- Provision of an accessible and safe network for human-powered transportation
- Links to key destinations within the community
- Encouragement of active and healthy living
- Reduction of Green House Gas emissions
- Reduction of traffic congestion

Additional targets are as follows:

- 15% of all residents will commute using an active mode by 2030
- 25% of all residents will commute using an active mode by 2040
- The Town will increase the total km of multi-use trails by 5 km by 2030
- The Town will install 5 km of dedicated bike lanes by 2025
- The Town will install 10 km of dedicated bike lanes by 2030
- The Town will design new trails and routes to connect to broader regional trail networks to support the development of a region wide trails network

For the ATP to be realistic and achievable, the plan must reflect community values and priorities. Using workshops, surveys, and interactive mapping the community was invited to offer ideas and comment on the active transportation and the ATP priorities, which helped the plan to:

- Address safety and mobility concerns.
- Provide input on the current state of walking and cycling in the community, and for the existing and desired informal and formal networks.
- Identify destinations and connectivity
- Prioritize investments to encourage and enable active transportation

This ATP is based on the community consultation and community profile of the Town of Port McNeill. The ATP responds to the community goals within the OCP and aligns with B.C.s Active Transportation Strategy.

The ATP, through the consultation process and data collection, identifies the current state of active transportation, safety concerns, kilometers of active transportation infrastructure, and potential Green House Gas (GHG) emissions reductions.

The ATP and Implementation is based on the existing and proposed active transportation infrastructure with consideration of maintenance post construction. The Implementation Plan and prioritization of phases is based on community input and best practices.

The capital projects and potential for grant funded projects has been identified within this ATP.

1.0 Introduction

The Town of Port McNeill is a "Forest Friendly" community and prides itself on having room to dream and grow. It's a scenic waterfront town on the east coast of northern Vancouver Island and possesses incredible scenic beauty. The Town, with a population of 2,337 residents, is rich in recreational assets that provide destinations in and around the Town, making it a great place for walking and cycling.

The Town does not have a formal active transportation network. The main infrastructure consists of sidewalks, stairway connections, and information trails. There is 13.65 km of informal bicycle routes and 9.5 km of sidewalks. These routes connect employment lands, commercial and industrial nodes, residential properties and school and recreation nodes.

Port McNeill is taking steps to implement formal pedestrian and bicycle facilities in the community and is committed to healthy living. This is evident in the importance of active transportation in relation to livability. As the Town develops policies, this plan recommends that active transportation is integrated into the new policies using the guiding principles, goals and objectives to improve walking, cycling and other active mobility options and route networks.

A comprehensive public process supports the review and development of the ATP. This planning process has occurred jointly with the Town's OCP and Zoning Bylaw review. The ATP will guide the Town of Port McNeill's investments in active transportation infrastructure as an official policy document. The plan establishes the vision, goals, and targets for improvements to active transportation. Strategies for achieving the goals of the plan and for implementation are included in this plan.

The ATP will assist the Town to promote walking and cycling, reduce automobile dependence and GHG emissions, increase physical activity, and improve public health outcomes, increase social connections, and reduce infrastructure demands.

1.1 What is Active Transportation?

Active transportation is all forms of non-motorized methods for traveling about. This can include:

- walking
- biking
- skateboarding
- in-line skating/rollerblading
- jogging and running

- · non-mechanized wheel chairing
- snowshoeing and cross-country skiing
- paddle boarding, kayaking, canoeing

Active transportation has many benefits to a community including healthy living and GHG emissions reduction. Walking and cycling are the most popular forms of active transportation but there are other forms including skateboarding, wheeling, in-line skating, mobility aids and riding the bus. Mobility aids include electric wheelchairs and electric assisted bicycles and scooters.

1.2 What are the Barriers to Active Transportation?

Safety

Safety is the main barrier to more people using active modes of transportation. The threat or perceived threat of being struck by a vehicle whether while walking at a cross wall or riding along a road can be scary and will deter many residents.

Accessibility

Another barrier is access to appropriate infrastructure. Lack of sidewalks, bike lanes or trails that are safe and attractive to use will keep people from choosing an active form of transportation. Infrastructure that is poorly designed that hinders use, such at curb cuts, are a barrier that wheelchairs or strollers cannot mount.

Geography

The topography in the Town is characterized by a north-east slope, leading from the highway to the waterfront. This slope can make moving around the community a challenge to some people.



Weather

Living on the west coast of British Columbia means that there is a lot of rain in the winter. This is a major deterrence for active transportation due to perceptions of comfort and convenience during winter and shoulder season weather.

1.3 What is the Purpose of an Active Transportation Plan?

The Town is developing the ATP to provide clear goals and strategies to support community goals of the OCP and to provide better connectivity from the neighbourhoods to key destinations in the community. The goals of the Port McNeill Active Transportation Network are to:

- Provide an accessible and safe network for human-powered transportation
- Link key destinations within the community

- Encourage active and healthy living
- Reduce Green House Gas emissions
- Reduce traffic congestion

This plan is intended to guide the on-going integration of active transportation infrastructure into the Town. The integration of the new infrastructure will be informed by the following objectives:

- 15% of all residents will commute using an active mode by 2030
- 25% of all residents will commute using an active mode by 2040
- The Town will increase the total km of multi-use trails by 5 km by 2030
- The Town install 5 km of dedicated bike lanes by 2025
- The Town install 10 km of dedicated bike lanes by 2030
- The Town will design new trails and routes to connect to broader regional trail networks to support the development of a region wide trails network

2.0 Background

2.1 B.C Active Transportation Strategy

The BC Active Transportation Strategy was released in June of 2019. The intent if the strategy is to change how British Columbians get around by promoting active transportation planning and projects. This shift in transportation modes has many benefits including:

- GHG emissions reduction
- Healthier populations
- More beautiful communities

The primary goal to double the number of trips using active modes of transportation are support by the following principles:

- Active transportation should be safe, easy and convenient
- Needs to be accessible to all abilities
- Needs to be integrated into the community's policy and planning

These principles have been adopted by the Town of Port McNeill as the guiding principles of the ATP and are reflected in the Purpose Statement outlined in section 1.3.

2.2 Community Profile

The demographic summary, housing profile, and labour force/employment information is included in Appendix 1 of this report and summarized in the following points.

2.2.1 POPULATION

The Port McNeill's permanent population has been declining. The decline in population between 2011 and 2016 is provided in *Figure 1*.

Table 1: Population comparisons

Location	2011	2016	% change
Port McNeill	2,505	2,337	-6.7%
Regional District of Mount Waddington	11,506	11,035	-4.1%
British Columbia	4,400,057	4,648,055	5.6%

Source: Statistics Canada, Census Data 2011-2016.

2.2.2 ECONOMIC DRIVERS

Historically, the Town's economy has been based in natural resources. It is a service hub for the region based on its marine location. Port McNeill's economy also includes government services, small businesses, manufacturing, and tourism.

There are economic benefits to active transportation as it can contribute to the development of a healthy and diverse local economy. It encourages residents to support local businesses, attract more visitors, provide choice for travelling to work, and decrease traffic congestion.

2.2.3 GEOGRAPHY

The Town of Port McNeill, located on the east coast of north Vancouver Island, is within the Regional District of Mt. Waddington (RDMW). The Town covers 13.77 sq km of area between Island Highway 19 and the east coast of the Island, looking out to the Broughton Strait. The Mainland of B.C. is across the Queen Charlotte Strait to the east.

In total, there is 13.65 km of informal bicycle routes and 9.5 km of sidewalks. The topography supports an approximate north to south road network in a modified grid. The north to south roads are typically steep within the Town and slope up to the south to the Island Highway from the shoreline.

The layout of the Town lends itself to easy connectivity between key destinations. The primary destinations in a town are the employment nodes:

Hospital

- Schools
- Downtown businesses

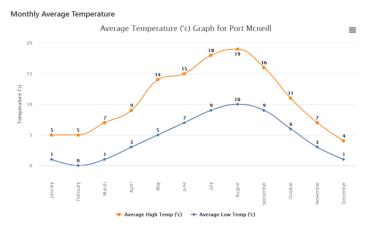
And key commercial and entertainment nodes:

- Pool and Recreation Centre
- Downtown
- Waterfront

The residential land use is in the south-west of the Town and north-east, including waterfront areas of the community.

2.2.4 WEATHER

Vancouver Island is a temperate climate with the mild, wet winters typical of the west coastal region. The average high and low temperatures are shown in Figure 2 and range between 4 degrees Celsius and 19 degrees, while the average low temperatures range between 0 and 10 through the year.



Source: data retrieved from worldweatheronline.com

Figure 2. Monthly Average Temperature

2.3 Consultation for Development of the Active Transportation Plan

The ATP was developed over a four-phase process that spanned over eight months beginning in August of 2020. The development of the plan involved exploring options with the community members and stakeholders, sharing initial results, gathering and reviewing community input, refining the content then creating a final plan. The vision and goals identified by the community have formed this plan.

The ATP consultation process included:

- 1. Vertisee. An on-line interactive engagement map was utilized to inform the ATP. Figure 3 shows how feedback was left for areas of trails, sidewalks, and destinations with the ability to interact with others on the map and leave comments in response.
- 2. Interactive mapping/workbook session were held in late 2020 where the community identified informal bike and walking trails.
- 3. Two Community Surveys, which were available online and in hard copy.



The intent for public engagement was to hold community walking/biking events. However, 2020 presented challenges and restrictions to inperson gatherings. This was the Town's opportunity to engage the community online. The interactive mapping was available to the community and a tool that enabled engagement with each other and the Town.

Online engagement tools and surveys were actively promoted to the community on social media and in news releases from the Town of Port McNeill. A total of 302 surveys for the OCP consultation and 65 for the ATP were received. The results are found in Appendix 2.



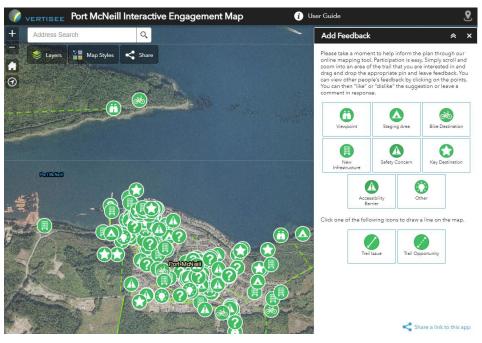


Figure 3. Vertisee online interactive map.

2.4 Shaping the Strategies

2.4.1 OFFICIAL COMMUNIYT PLAN AND REGIONAL POLICY CONTEXT

The Town of Port McNeill falls within the RDMW. The District has developed a Regional Plan which has been used as general guidance for a framework for the OCP policy review.

2.4.2 REGIONAL POLICY FRAMEWORK

The RDMW Regional Plan focuses on the reduction of GHG, the efficient use of land the development of a well-connected transportation system. The proposed ATP network will contribute to the RDMW objectives by supporting the goal for efficient transportation systems and will contribute the GHG reductions over time.

Green House Gas

To support development of efficient and effective transportation and communication services that provide long term and efficient linkages within the RDMW and beyond.

2.4.3 OFFICIAL COMMUNITY PLAN

The Town is currently going through the OCP review process, and the ATP public consultation has been completed concurrently. Policies have been recommended for the parks, recreation, and transportation sections to reflect the findings of the consultation process. These key findings will be integrated into the OCP to ensure consistency with the ATP. Further, the Town has signed onto the Climate Action Charter. Policy recommendations are found in Section 5 of this plan.

2.5 Data Collection

Data was collected from numerous sources in support of the ATP. The data included a review of:

- The current state of pedestrian and cycling infrastructure a
- Cumulative kilometers of active transportation infrastructure,
- Vehicle incidents and safety concerns; and,
- potential GHG emissions reductions.

In addition to, the statistical information listed above, comments from the public consultation informed the overall process. The research findings, analysis and public input is included throughout the ATP. This data is summarized in Appendix 3.

2.6 Potential Green House Gas Emissions Reductions

The current active transportation mode share (a transportation mode is the way a person move around) in Port McNeill is reported at 10% or ~ 230 people (see Appendix 3). The goal is to have 15% of the community using an active mode by 2030. For calculations of GHG reductions the current active mode

share of $10\% \sim 230$ individuals was used. Based on the goals and assumptions, an increase of 120 individuals will utilize active modes by 2030.

For the calculation we have assumed the following based on 2014 BC Best Practices Methodology for Quantifying Greenhouse Gas Emissions Including Guidance For Public Sector Organizations, Local Governments And Community Emissions. We have used the Light Duty Truck vehicle emission in the following calculations:

Table 2: GHG emission calculations assumptions

Average vehicle distance traveled	15,000km/year
Light duty truck vehicle	2.353 Kg/L COe (MoE, 2016)
Average fuel economy	8.9 L/100 km source (Canada Energy Regulator, accessed on Jan
	14, 2021)

It is assumed that due to the climate in Port McNeill most people will use a vehicle to commute during the winter and rainy season. The following table summarizes the potential COe (carbon dioxide equivalents) reduction based on the four scenarios for active transportation commuter's active mode utilization levels.

Table 3: Potential GHG emissions reductions

% of Trips	Tonnes of COe/year/vehicle	Total potential reduction (tonnes) per year
100% shift scenario –	3.1	372
75% shift Scenario	2.3	276
50% Shift scenario -	1.55	186
25% shift scenario	0.78	93.6

Sample calculations can be found in Appendix 3.

Based on the calculations above, the likely GHG reduction from an increase in active mode share will result a 186 tonnes of COe reduction per year by 2030.

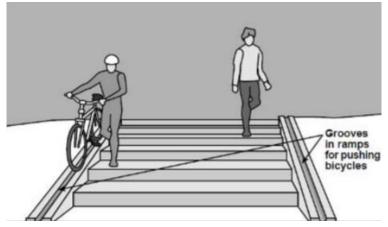
3.0 Active Transportation Plan Network

3.1 Existing Network

3.1.1 ROADS AND STAIRS

The locations of the existing sidewalks and stairways within the Town of Port McNeill are found in Figure 5. The locations are not accessible for some of the community, and the topography makes accessibility a challenge in some of the mapped locations.

Solutions to inaccessible stair routes include bike tracks on the sides of the stairways (Figure 4.), however the costs associated with this type of construction must be weighed against possibly re-routing or alternatives.



Source: Oregon Bicycle and Pedestrian Plan

Figure 4. Bike Ramps on Stairs

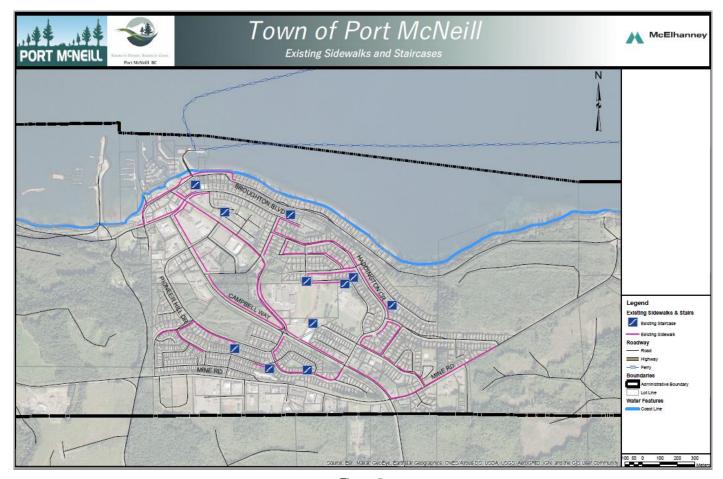


Figure 5.

35% of the residential areas in Port McNeill have existing sidewalks.

3.1.2 Public Trails & Private Land Trails

Trails and the networks they form are either a formal or an informal, yet critically integral, part of the active transportation network. They provide important connections between places and spaces. However, it's been found that just one in four people reported that they were aware of how many multi-purpose trails were in their jurisdiction. Figure 6 locates the existing formal and informal trails in the Town of Port McNeill. Currently, there is only one formal trail within the Town, which has been located on the map.

Recreational trails serve multiple purposes to the public, such as allowing users access to enjoy scenic vantage points during a relaxing walk or a strenuous hike, an adrenaline filled rush in serenity and solitude during a physical activity like mountain biking, or just a place to relax away from the person's home or place of occupation.

Transportation desire trails are informal routes used by the public that are not planned. Reasons that "desire line" transportation trails are created include users that are deterred from the existing infrastructure because of one, or a combination of the following:

- traffic and the proximity to moving vehicles and potential injuries
- sidewalk hazards associated with colder and/or wetter climates
- Infrastructure availability such as sidewalks, gravel shoulders, or bicycle lanes
- maintenance of non-vehicular infrastructure
- distance between two destinations along the road grid system

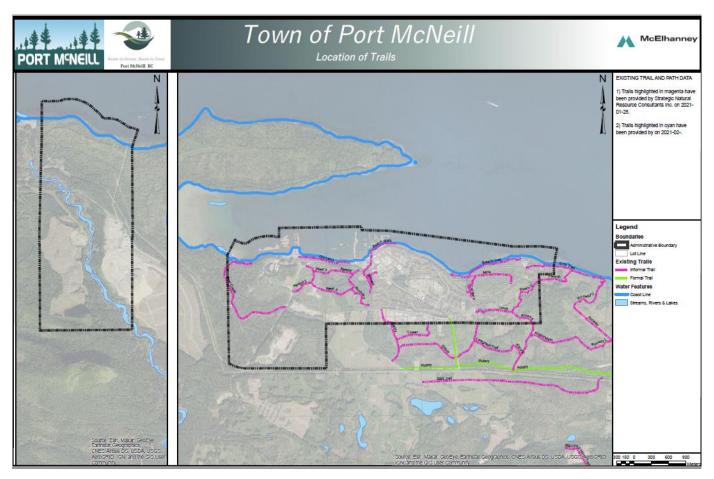


Figure 6.

There are environmental benefits from utilizing transportation trails as a substitute for vehicle trips, especially short trips. Active transportation networks result in a reduction in pollution including air pollution from combustion of petroleum-based fuel products, water pollution, (road runoff containing fluids and wastes from vehicles), and a reduction in land area required to facilitate vehicle parking.

Studies have shown that users of both recreational and transportation trail systems show signs of being healthier than their non-user counterparts. A study by the National Collaborating Center for Environmental Health showed that active transportation users, including trail users, had a reduced risk of a variety of illnesses including coronary heart disease, obesity, depression, and high blood pressure. An additional study by the World Health Organization found that users who ventured away from roadways and traffic

showed decreased levels of noise and noise-related stress (i.e., tires on asphalt, engine & acceleration sounds, horns, and brakes), and other illnesses associated with physical inactivity.

Economically, trail systems provide a benefit over traditional sidewalk systems in that they create a more direct route for commuters to their place of employment. Active transportation can reduce vehicle congestion, and as a result, the local municipality or regional district may experience reduced maintenance costs and extended life cycle of roadways. Users of active transportation can decrease vehicle maintenance costs and reduce healthcare related costs from pollution related illnesses or intermodal injuries such as bicycle versus car collisions. Additionally, studies have found that property adjacent to trails see an increase in value as opposed to those that are not near a trail or trail network.

Trail networks can provide cycling, walking, hiking and other eco-tourism friendly activities. There is a growing demand for sense of space and pristine or untouched wilderness. These locations have seen growth in the hospitality industries as a result, and tax money has a direct impact on the local Municipality or Regional District.

3.1.3 EXISTING PORT MCNEILL TRAIL NETWORK

The focus of the ATP is on public owned lands, however informal trails provide key connectivity to the active transportation network. Port McNeill has only a single trail that appears to be on public land as noted in Figure 6. However, there are significant number of trails on private lands that are used by the public. The trails throughout the community have inconsistent shapes and physical forms, lack cohesion, and legal user agreements. The existing known trails within the Town's boundaries have been captured in Table 3.

Table 4: Existing Trails and Ownership

Name	Length	Ownership
Barb's	0.3km	Private
Beach Trail	0.8 km	Private
Beach Walk	0.7 km	Private
Bear Creek	2.4 km	Private
Cardena	0.6 km	Private
Dry Land	0.9 km	Private
Lions	0.7 km	Private
Mine	1.2 km	Private
New Pit	0.7 km	Private
New Pit 2	0.4 km	Private
Pete's U	1.4 km	Private
Rotary	0.1 km	Public
Runners	0.7 km	Private
STP	0.5 km	Private
School House	1.2 km	Private
West 3	1.2 km	Private

^{*} Note: The trail routes names in Figure 6 and route locations shown in the accompanying map in Figure 6 below are courtesy of Strategic Natural Resource Consultants Inc.

A review of the Town of Port McNeill's bylaws found a single reference to trails in section 2403 of *Part 24* – *Comprehensive Development Zone 1 (CD-1)* of the Zoning Bylaw (Bylaw No. 671, 2016). The statement is as follows:

2403 Permitted Principal Uses

- a) Single-family and two-family dwelling units, including factory-built homes but excluding manufactured homes, that meet the requirements of section 2405
- b) Parks, nature parks and playgrounds,
- c) Trails, paths, and greenways.

Only the Rotary Trail is a formal trail on public land. The trail was constructed of building material waste from the construction phase for Windsor Plywood. This trail appears to stop at the southern municipal boundary and does not appear to have wayfinding or trail entrance signage.

3.1.4 FUTURE PORT McNeill Trail Network

Port McNeill does not currently possess a Trails Master Plan document or trail specifications that can be integrated into the OCP. The purpose of a Trails Master Plan document is to:

- Involve all stakeholders in the creation of a future trail network within the Town
- Provide a clear and concise direction for the Town's elected officials to invest and expand the existing trail network in the future
- Close gaps in an existing trail network and improve connectivity between destinations
- Create safer and more accessible trails usable by all trail users
- Enhance and upgrade existing trail facilities to current or future standards

When being developed, trails and trail networks should be designed in a consistent manner with standardized specifications to ensure the network provides connectivity through cohesion and conformance. As an example, Parks Canada utilizes a "Best Practices" approach to creating their trail network and defines this method as:

"Best Practices" refer to trails, and their components, that represent good examples of trail planning and design techniques that show respect for ecological and commemorative integrity, appropriate visitor opportunities, and sustainable practices. These trails should contribute to the presentation of heritage themes and messages, help minimize conflict between users, and have minimal environmental impact upon the heritage settings.

Trails that employ "Best Practices" possess the following qualities: pleasing aesthetics, solid construction using acceptable materials, informative offerings, and safety. They are provided with a wise and efficient use of resources. These trails are in scale with, complementary to, and appropriate for their natural and cultural surroundings..."

Parks and Trails have been mapped with assistance from the mountain bike club. The informal trails will, require a system / process for the Town to formalize these trails over time. Cyclists use the informal routes on private land extensively.

3.2 Destinations

It is essential when planning for active transportation that the proposed routes provide connectivity to key destinations. These destinations can be the key employment areas, schools, tourist attractions, or primary recreational attractions to encourage travel using active modes for both commuting and recreational purposes. The Destinations Map in Figure 7 illustrates the primary areas that people travel to and between.

3.2.1 COMMERCIAL & EMPLOYMENT NODES

Downtown connection to outlying residential areas is important to enable active transportation and has a major influence on its use. Town centers surrounded by residential areas attract trips suitable for active transportation. Urban form and the built environment have an influence on active transportation use. Mixeduse areas produce the most walking trips.

Employment centers in the Town include:

Schools

Hospital

Industrial / Commercial areas





Employment centers are key in the potential growth area for cycling trips. The schools, hospital, ferry, marina, and services are destinations that active transportation infrastructure can address.

3.2.2 SCHOOLS AND RECREATIONAL NODES

The school and recreational nodes are a key destination for the residents of the Town and centrally located, which presents opportunity for active transportation networks.

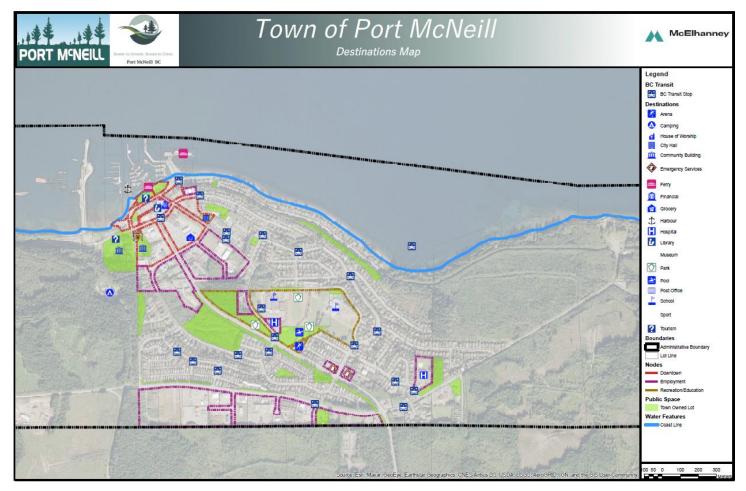


Figure 7.

Local transit stops require effective walking connections with continuous accessible sidewalks. Transit nodes benefit from active transportation feeder links, while gaps in infrastructure network is an impediment to active transportation trips.

3.3 Networks and Classification of Routes

The routes identified through research and public consultation consist of primary, secondary, and tertiary. The route hierarch is mapped in Figure 8, which provides the main active transportation network locations and classify them based on the intensity of use.

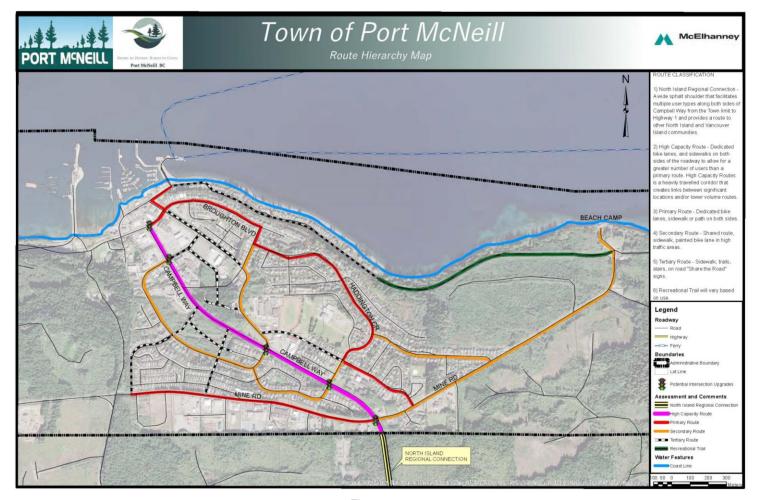


Figure 8.

3.4 Standards

The active transportation standards are based on the B.C. Active Transportation Design Guide. The standards have been identified based on the route classification systems in Figure 8.

The recommended standards are to provide guidance during future design and construction of the active transportation network. The recommendations are based on an assessment of the proposed routes and the needs of the community. During detailed design, these standards should be used as the basis for design, however, the standards may need to be modified based on the physical constraints of a site. Where standards need to be modified, the designing engineer should utilize B.C. Active Transportation Design Guide Standards and Transportation Association of Canada (TAC) to develop a solution that meets the intent of the standards.

3.5 Primary Routes

Primary routes are intended to act as the backbone for the active transportation network. These routes are to provide the main connection to key destinations throughout the Town and link most neighbourhoods

through connection to the second and tertiary routes. The primary routes are made up of on-road and offroad infrastructure. Primary routes include:

- Broughton/Haddington/Kingcome Corridor
- Campbell Way Multi-use Trail
- Mine Road Corridor/Multi-use trail

The primary routes may include multi-use pathways as shown in Figures 9 and 10, with buffers between a vehicle lane and the multi-use trail.

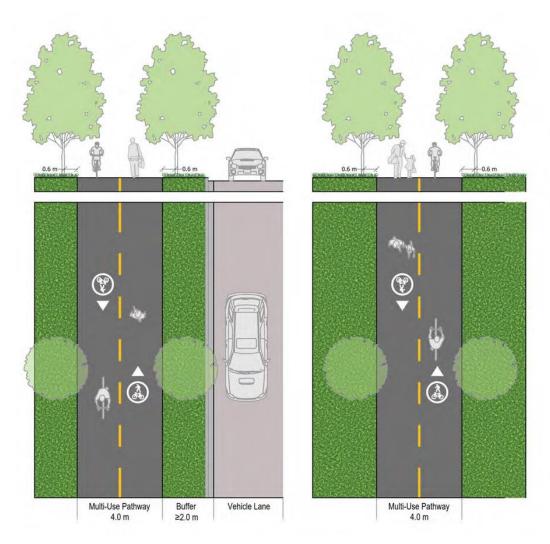


Figure 9. Design of Multi-use pathway in cross-section and plan view with widths and key features. Reprinted from B.C. Active Transportation Design Guide, by Ministry of Transportation and Infrastructure. Retrieved from https://www2.gov.bc.ca/gov/content/home

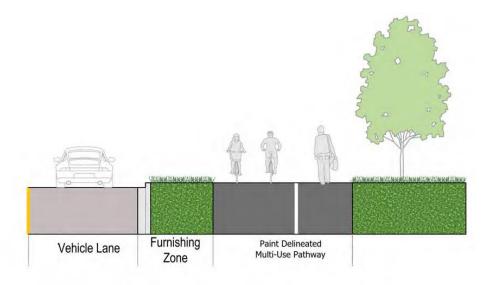


Figure 10. Recommended Campbell Way and Mine Road Multi-use trail standard Reprinted from B.C. Active Transportation Design Guide, by Ministry of Transportation and Infrastructure. Retrieved from https://www2.gov.bc.ca/gov/content/home

3.6 Secondary Routes

Secondary routes are intended to connect neighbourhoods to the main corridors. They are routes located along known corridors but may not provide connection to key destinations. Secondary routes are primarily on-road and consist of bike lanes and signage to denote a bike route as shown in Figure 11. These can occur in areas with or without sidewalks. Secondary Routes include:

- Pioneer Hill Dr./Chelohsin Cres
- Haddington Cres (from Woodland to Campbell way)
- McNeill Rd./Cedar St.
- Mine Rd (from Beach Camp to Campbell Way)

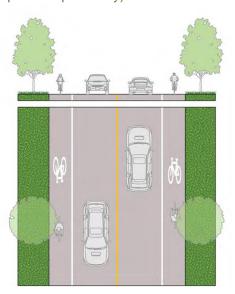


Figure 11. Recommended on-road secondary route configuration. Reprinted from B.C. Active Transportation Design Guide, by Ministry of Transportation and Infrastructure. Retrieved from https://www2.gov.bc.ca/gov/content/home

3.7 Tertiary Routes

Tertiary routes are routes of convenience that tie internal neighbourhood routes together and provide additional access and connections. These are located internal to neighborhoods. They typically will be located on low-speed neighborhood roads. The routes will be signed with on-street "share the road" markings to denote the routes. The tertiary routes are:

- McNeill Rd. (Cedar to Broughton)
- Hemlock St.
- Argus Dr. corridor
- Betts Blvd.
- Beach Dr.

3.8 Trails

Port McNeill has numerous formal and informal trials throughout the community. One trail is on Town owned land and there is a significant number of trails on private lands. The focus of the ATP is on public owned lands, however informal trails provide key connectivity to the active transportation network. When developed, trails should be designed in a consistent manner to ensure the network provides accessible connections and opportunities to move around a town without the use of a vehicle.

Table 5: Trails Classification System

	TRAIL TYPE			
	TYPE I	TYPE II	TYPE III	TYPE IV
Trail descriptor	Trail wide (offers side-by- side travel), firm, stable and uniform. Trail is relatively flat with no obstacles.	Trail relatively smooth, moderate width. Trail flat to gently rolling and may have short steep sections. Some obstacles may be present.	Trail continuous but narrow, rough. Trail may be steep, and obstacles may be common.	Trail undeveloped, unmaintained
Trail rating	Easy	Easy to difficult	Moderate to extremely difficult	Unrated
Targeted visitor	Family-friendly, suitable for all visitors	Suitable for most visitors who are generally active	Suitable for visitors who are active and have trail experience	Untargeted
Type of Experience	Developed	Semi-developed	Natural	N/A
Level of Use	High	High to Moderate	High to Low	Low
Tread width (typical average)	1.5 - 3.0m	1.0 - 2.0m	0.3 - 1.0m	0.3 - 0.6m
Clearing width	0.5m on each side of trail	0.5m on each side of trail	1m total clearance	N/A
Clearing height	3.5m	2.5 - 3.5m	2.5m	N/A
Surface treatment	Packed screenings, pavement, boardwalk	Packed screenings, native mineral soils	Native mineral soils	Unsurfaced
Obstacles	No obstacles present, no stairs, may be fully accessible	Obstacles infrequent, stairs may be present	Obstacles may be common or intended for challenge, stairs may be present	N/A
Signage	Interpretive and directional signage where appropriate (max information provided)	Interpretive and directional signage where appropriate (moderate information provided)	Limited signage (minimal information provided)	No signage (no information provided)
Grade (general)	Flat to gently rolling	Flat to gently rolling with short steep sections	Rolling with steep sections	N/A

Typical Cross Sections for Trail Implementation

There are multiple variations of trail construction standards based on the intended use. Typical trail sections have been provided in Appendix 4. These designs have integrated key design criteria such as universal design from BC Active Transportation Design Guide. The sections also consider the following:

Trails on slopes

There are slopes within the Town of Port McNeill which will have to be addressed in the trail design. Typically, cut slopes are created to deal with grading for the trailway and drainage.

Multi-use trails

Trails intended for use and shared by pedestrians and cyclists require space for both users. A typical width for a multi-use trail may be 3 to 4 meters.

Walking paths

Pedestrian trails can be paved or unpaved with hard packed surfacing for access and safety.

Mountain biking/hiking paths

Mountain biking and hiking paths/trails can be informal and unpaved with hard packed surfacing. Trail etiquette for shared trails can have signage to indicate the "rules of the trail" for yielding appropriately and avoiding conflicts.

CROSSINGS

Crosswalks and intersection are the main conflict point between active transpiration and vehicles. The BC Active Transportation Guide provides recommended designs for crossings such as pedestrian crossing in residential neighbourhoods and multi-use paths on residential streets. These specific designs are found in Appendix 4.

4.0 Active Transportation Plan Implementation

The Town of Port McNeill is poised to implement the ATP with the priorities for the active transportation network identified through assessment and public consultation. The priority projects are shown in Figure 12.

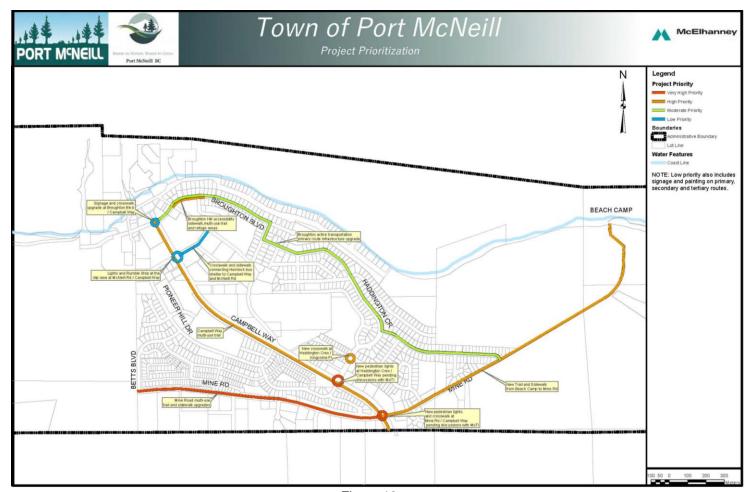


Figure 12.

4.1 Priority Projects

The projects listed in Table 3 and 4 were identified through the route assessment / classification process and the community consultation process. The routes and projects were then prioritized through public consultation and are listed in Table 5.

Table 6. Prioritized Active Transportation Projects

	Prioritized Projects
1	Mine Rd. Multiuse trail/Sidewalk Upgrades
2	Pedestrian lights and crosswalk at Mine Rd/Campbell way
3	Pedestrian lights at Haddingtion/Campbell Way
4	Broughton Hill accessibility sidewalk/multiuse with refuge areas
5	Trail/sidewalk connection from Beach Camp to Woodland on Mine Road
6	Campbell Way Multiuse trail
7	Crosswalk at Haddington and Kingcome
8	Broughton active transportation Primary Route infrastructure Upgrades
9	Signage and cross walk upgrade at Broughton/Campbell Way intersection
10	Cross walk, and sidewalk connecting Hemlock bus shelter to Campbell Way and McNeill Rd
11	Lights/rumble strip at slip lane McNeill Rd./Campbell Way
12	Signage and painting on primary, secondary, and tertiary routes

Additional projects that were consistently identified in the consultation process were:

- To upgrade Pioneer Hill sidewalk
- Improvements to Beach Drive
- To improve routes for school children

The community was asked to choose one Capital Project from the listed large, moderate, and low-capital projects as a priority. Table 6 lists the projects as large capital projects, moderate capital projects and low capital projects in order of priority to the community.

Table 7. Prioritized Active Transportation Projects categorized based on anticipated capital cost

	Large Capital Projects (Grant Projects) Over 500K
1	Mine Rd. Multiuse trail/Sidewalk Upgrades
2	Trail/sidewalk connection from Beach Camp to Woodland on Mine Road
3	Campbell Way Multiuse trail
4	Broughton Hill accessibility sidewalk/multiuse with refuge areas
5	Broughton AT Primary Route infrastructure Upgrades

	Moderate Capital (Capital Plan or Grant) Over 100 K
1	Pedestrian lights and crosswalk at Mine Rd/Campbell way
2	Pedestrian lights at Haddington/Campbell Way
3	Cross walk, and sidewalk connecting Hemlock bus shelter to Campbell Way and McNeill Rd
4	Crosswalk at Haddington and Kingcome
5	Lights/rumble strip at slip lane McNeill Rd./Campbell Way
	Low Capital (Capital and Operation and Maintenance) Under 100K
1	Signage and cross walk upgrade at Broughton/Campbell Way intersection
2	Signage and painting on primary, secondary, and tertiary routes

The moderate and low capital projects are relatively easy to plan for and are included in the 10-year capital plan for the Town, whereas the large capital project will likely require partnerships and funding from other sources to develop. The projects were prioritized based on community desire. The top three large capital projects were assessed, and class 'C' cost estimates were developed to support future infrastructure grant applications (Appendix 5).

4.2 Funding Options

There are various funding options available to local governments to fund the construction of active transportation infrastructure. These funding sources include:

- Development Cost Charges
- Community Amenity Contributions
- Capital Projects/Taxation
- Infrastructure Grants

4.2.1 DEVELOPMENT COST CHARGES

Development cost charges (DCCs) are enabled through Section 559 of the Local Government Act. These charges are fees that are applied to all new development based on the developments impact on a infrastructure that results in an increased demand from new development. DCCs can be required to fund new roads and road improvements, including the integration of active transportation infrastructure within a road. To do so, a detailed engineering review must be completed to cost out the new infrastructure and then to allocate what percentage of the new infrastructure would be due to new development. This is then allocated as a charge on all new development to help fund future infrastructure improvements.

4.2.2 COMMUNITY AMENITY CONTRIBUTIONS

Community amenity contributions (CACs) are a tool that local governments can suggest to developers to help address impact of a new development on a community or neighbourhood. CACs are voluntary contributions by a developer to a community as part of a new development proposal and are negotiated at time of a rezoning or OCP amendment. CACs are not mandatory or a requirement of the local government. They are established in a policy to outline the types of amenities that would benefit a community and address the impacts of new development. This tool can be used to promote the investment in active transportation by developers by including active transportation infrastructure within the CACs policy.

4.2.3 TAXATION/CAPITAL PROJECT PLANNING

General taxation and capital planning are the primary methods for a local government infrastructure improvement. The Town of Port McNeill already has established a sidewalk improvement program through its capital planning process allocating \$60,000 per year to expanding the sidewalks within the community. The same program can be expanded to include other forms of active transportation infrastructure such as bike lanes, signage, and trail improvements.

The Town should incorporate the ATP recommendations into its short-, medium-, and long-term financial plans to ensure that projects are accounted for in the Town's capital planning process.

4.2.4 GRANTS

Grant programs are an effective way to construct infrastructure projects in a community. To take advantage of grant programs it is typically required that the preliminary planning and costing of a project is complete. Currently, there are numerous granting programs for infrastructure construction, including active transportation specific granting programs, (see Appendix 6) such as:

- BC Active Transportation Infrastructure Grant Program
- COVID-19 Resilience Infrastructure Stream
- Investing in Canada Infrastructure Program
- Community Economic Recovery Infrastructure Program
- Community Health Community program

These programs are effective to larger capital projects that are not possible under short term budget and require long-term saving or borrowing to complete. As outlined above, Class "C" estimates were provided for the top two prioritized projects to support future grant program applications.

It is recommended that the Town focuses on the use of long-term capital project planning (taxation) and infrastructure grant programs to facilitate the construction of the active transportation network. The other funding options, although valid, are not recommended at this time as they result in increased fees to development, which can result in a disincentive to development. The OCP process has illustrated the need

for new development and new and increasing fees may have a negative impact on attractive new development. The other funding option should be revisited during the next OCP update process.

4.3 Construction Schedules

Projects will be completed as grant funding is available. Small projects will be completed under planned Town capital projects and programs.

4.4 Maintenance Plan

A monitoring strategy is essential to ensure that the ATP is implemented as intended and making progress towards the vision, goals, and targets. It will enable the Town to allocate resources to implement prioritized initiatives.

All active transportation routes will be added to the bi-annual sidewalks inspections carried out by the Town's Public Works team.

The Towns annual maintenance budget is to cover:

- Paving
- Concrete works
- Line painting
- Drainage
- Painting handrails,
- Landscaping
- Gravel works
- Electrical maintenance

5.0 Recommendations

The following are recommendations and actions to maintain and implement and develop the ATP and active transportation network in the future.

5.1 Policy Recommendations for Implementation

1. Active transportation should be integrated throughout the OCP.

The following polices should be integrated into the Official Community Plan.

Healthy and Active Living

Recreation Objective: To promote healthy living

Policy: Encourage commuting using active modes of transportation. This will be achieved by actively working to implement the active transportation plan.

Parks Objective: <u>To create a comprehensive trail network that connects on-road and off-road active</u> transportation routes

Policy: Council will work with private landowners to establish public access trails. This will be done using access agreements, memorandum of understanding and where possible the dedication of trails to the Town during subdivision approvals.

Active Transportation Targets

Transportation Objective: <u>To have 25% of the population community by active modes by 2040</u> Policy: The Town will work with the Regional district to promote trail connectivity between Port McNeill and adjacent rural communities such as Nimpkish Heights and Hyde Creek.

Policy: The Town will work with private landowners to formalize existing trails systems in support of the community's active transportation and recreational trail network.

Policy: The Town will work with Ministry of Transportation and Infrastructure to ensure universal accessibility to all cross walks and curb cuts across Campbell Way.

Policy: The Town will actively seek grant and funding programs to support the expansion of the active transportation network. This will Include partnership with non-for-profit groups and the Regional District.

Policy: The Town will consider active transport updates on routes identified in the active transportation plan when road upgrade projects are undertaken.

Climate and the Environment

Climate and Environment Objective: <u>Council will encourage the development and use of the active transportation.</u>

Policy: The Town will have 15% of the population using ATP by 2030 and 25% by 2040. This will be achieved by implementing the active transportation plan, through addition of accessible, safe and healthy options to move around the community.

2. Develop a program to formalize the trails on private lands.

There are several tools the town can use to begin formalizing trails on private lands. These include:

- Memorandums of understandings
- Occupancy agreements
- · Park land dedication
- Trails Master Plan

These options should be discussed with existing landowners to identify what solution will work for both the Town and landowner.

5.2 Program Recommendations

1. Develop an education program with key stakeholders and partners to promote active transportation network use.

A key element of an effective active transportation program is to provide education to the community. This increases awareness and helps to promote healthy living. An education program is a tool to help meet the goals set out in this plan.

5.3 Funding and Implementation Recommendations

1. Implement cost effective quick wins to promote the ATP and network.

Several of the identified projects will require significant capital investment and numerous years to implement, specifically the primary routes. It's recommended that the Town start promoting the active transportation network and delineate identified routes with signs and painted lines.

This will allow for some "quick" wins and enable education and the identification the network in the community. The following is recommended:

- a. Primary routesSignage and bike lanes painting
- b. Secondary routesSignage

This could be integrated into a program similar to the existing sidewalk expansion program.

2. Ensure that active transportation infrastructure expansion is included in all Town projects that require road works.

As the Town implements an asset replacement project, active transportation infrastructure should be added to all road upgrades on projects that require requires road works.

3. Seek out partnerships with first nations, local groups and the regional district to expand the connectivity of the network beyond the boundaries of the Town.

A key partnership for several of the projects will be with the Ministry of Transportation and Infrastructure as the control and jurisdiction of Campbell Way to the ferry falls under the Ministry's control. Four of the prioritized projects fall withing MOTI roads and will require their cooperation to implement. Further, the Rotary Trail extends from the boundary of the Town to the highway. This is an opportunity to create a regional network that will promote Port McNeill as the base camp for access to the North Island.

4. Continue to explore grant opportunities for all priority projects.

As noted in the funding section, there are several options to fund expansion of the active transportation network. Granting programs range in scope and financial support therefore it is recommended that the Town actively explore granting programs for the larger capital projects while being open to utilize grant opportunities that arise for infrastructure improvement to implement the "low hanging fruit" projects.

5. Apply for grants for prioritized large capital projects.

Class "C" estimates have been completed for the top three prioritized large capital projects (see Appendix 5). The Class "C" estimate should be used as the basis to apply for grant funding. As noted, there are several different grant programs in BC. Each have differing eligibility criteria and intake timing. Appendix 6 provides 5 different eligible grant programs.

Table 8. Policy Implementation

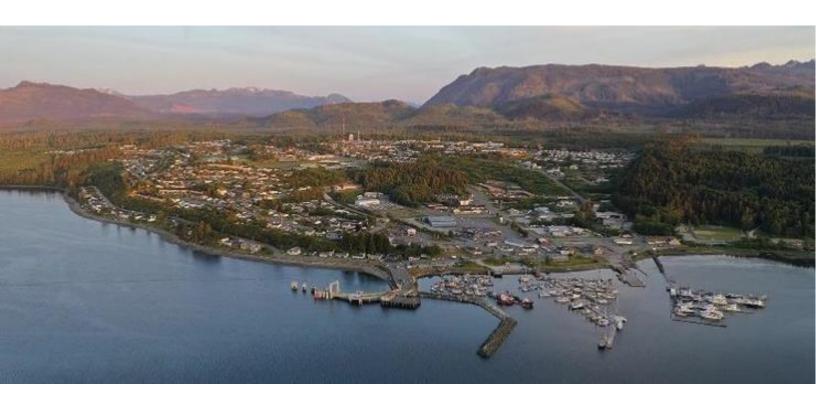
POLICY	Ease of	Cost	Priority	Notes
	Implementation			
AT should be integrated	Easy	Low	High	
throughout the OCP				
Develop a program to	Easy - Moderate	Moderate	Medium	
formalize the trails on				
private lands.				
Develop an education	Moderate	Low-moderate	medium	
program to promote AT				
network use.		84 1 /		
Implement cost	Moderate	Moderate	High	
effective quick wins to promote network.				
•	_			
Ensure that AT	Easy	Moderate to high	low	
infrastructure				
expansion is included in all Town projects that				
require road works.				
Seek out partnerships	Moderate	Low to moderate	Low-	
with local groups and	Moderate	Low to moderate	medium	
the regional district to			mediam	
expand the connectivity				
of the network beyond				
the boundaries of the				
Town				
Continue to explore	Moderate	Low	medium to	
grant opportunities for			high	
priority projects			_	
Apply for grants for top	Moderate to	Low to moderate	high	
priority capital projects	hard			

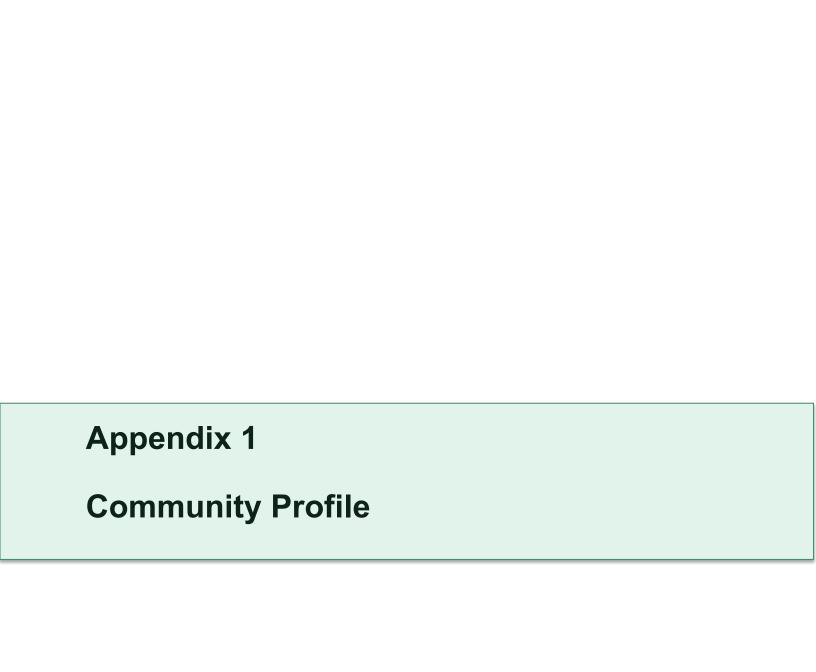
Town of Port McNeill



Room to Dream. Room to Grow.

Port McNeill BC







Room to Dream. Room to Grow.

TOWN OF PORT MCNEILL

Community Profile 2020

File: 2221-49324-00





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Introduction



1.1 BACKGROUND

The Town of Port McNeill (Port McNeill) Official Community Plan (OCP) adopted in 1997 requires updating which McElhanney Ltd. (McElhanney) will complete. This report provides information on the Community Profile, one of the key deliverables in the OCP update and Active Transportation Plan development.

1.2 PURPOSE

The Community Profile (Profile) is a background document that provides an information baseline of demographic, socio-economic, and housing data. The Profile will inform the OCP and Active Transportation Plan process moving forward including providing historical context and important background information for the public engagement process and in the development of OCP policies. The Profile has been compiled using the most recent Statistics Canada Census data (2016), data available from previous Censuses, and BC Stats.

The OCP is a comprehensive long-term plan for the physical development of a local government. The OCP must contain policies and maps around residential development and housing which includes affordable housing, rental housing, and special needs housing. The OCP also includes other land uses such as commercial, industrial, institutional, agriculture, recreational, and public utilities. Prior to developing these policies and maps, it is important to determine and understand the historical context of growth in a community and its current and potential growth including any changes to the demographic profile, housing, and labour force.

1.3 REPORT STRUCTURE

The Profile is organized into three chapters. Each chapter provides a summary of the data collected for Port McNeill with comparative data provided for the Regional District of Mount Waddington (RDMW), the province (BC), or both. At the end of each chapter, a summary is provided regarding the data and potential implications and directions with respect to the OCP (OCP implications). The three chapters included:

- Chapter 2, Demographic Profile past and current (2016) population, age, and household characteristic data.
- Chapter 3, Housing Profile past and current data on dwellings, housing types and tenure, and age
 of housing stock.
- Chapter 4, Labour Force and Employment past and current data on employment rates and employment by industry.

1.4 DATA

To better understand the data presented in the Profile, the following background information and considerations regarding the data is provided.

- Census Years The Census is conducted every five years by Statistics Canada in years ending in "1" and "6" (i.e., 2011, 2016). Data compilation takes time to complete and data is released by topic areas over the course of the year following the Census.
- Census Data: Two forms were used in the 2016 Census, a "short-form", and a "long-form". A sample of 25% of Canadian households received the long-form questionnaire, and 75% the short-form. Short-form questions included, date of birth, sex, relationship of household members, and languages spoken. In addition to the questions on the shortform, the long-form included questions regarding housing and labour force activity. In 2011, the Census did not include a long-form, therefore labour force data is either limited or not available.
- Data Availability: Given Port McNeill's size, information in several data categories has been suppressed (i.e., not published) by Statistics Canada. This is done for two reasons: 1. to ensure that the identity and characteristics of respondents is not disclosed to ensure the confidentiality of respondents is maintained and 2. to limit the dissemination of data of unacceptable quality. The thresholds for data suppression (i.e., sample size) is dependent on the type of data. Additionally, information from the Census only includes data regarding "usual residents" or permanent, full-time residents.



Demographic Profile

2.1 POPULATION

Recent population estimates for Port McNeill, the RDMW, and BC are provided in *Table 1* with the percent change between 2011 and 2016. While BC has been growing at a moderate rate, the RDMW and the Port McNeill's permanent population has been declining. Historical population between 1996 and 2016 is provided in *Figure 1*.

Table 1: Historical Population 2011-2016

Location	2011	2016	% change
Port McNeill	2,505	2,337	-6.7%
Regional District of Mount Waddington	11,506	11,035	-4.1%
British Columbia	4,400,057	4,648,055	5.6%

Source: Statistics Canada, Census Data 2011-2016.

20% Decrease Over Time

Figure 1: Port McNeill's Historical Population 1996-2016

Source: Statistics Canada, Census Data 1996-2016.

2.2 AGE CHARACTERISTICS

The average age of communities across Canada and BC is increasing; Port McNeill is no exception. According to Statistics Canada Census data, the median age of Port McNeill residents has increased slightly from 38.0 in 2011 to 40.7 in 2016 (*Table 2*). Approximately, 12% of the population of Port McNeill is over the age of 65 and 24% are under the age of 20. The largest proportions of residents are

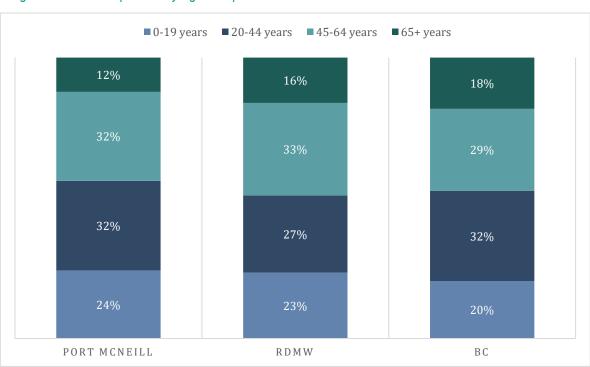
between 20-44 years (32%) and 45-64 years (32%) (*Figure 2*). This age distribution is similar when compared to the RDMW and BC which all have an even distribution of residents between age groups.

Table 2: Median Age, 2011-2016

Location	2011	2016
Port McNeill	38.0	40.7
Regional District of Mount Waddington	41.8	44.3
British Columbia	41.9	43.0

Source: Statistics Canada, Census 2016.

Figure 2: Percent Population by Age Group - 2016



Source: Statistics Canada, Census 2016.

2.3 HOUSEHOLD CHARACTERISTICS

Household characteristic data contains information about residents living within dwellings and includes number of people and household incomes. Households refer to a person or group of persons who occupy the same dwelling and do not have a usual place of residence elsewhere.

2.3.1 Average Household Size

Table 3: Average Number of Persons per Household – 2006, 2016

Location	2006	2011	2016
Port McNeill	2.5	2.5	2.3
Regional District of Mount Waddington	2.4	2.3	2.2
British Columbia	2.5	2.5	2.4

Average household sizes (persons per household) have been decreasing over the decades at all geographic levels. This trend is typical for aging populations that contain households without children or spouses. Port McNeill's household size has been averaging just above two persons over the past ten years, and as of the 2016 Census, is 2.3 persons per household (*Table 3*). This is similar to the RDMW and BC which are 2.2 and 2.4 respectively.

2.3.2 Households by Household Size

Table 4: Port McNeill Total Households by Household Size - 2011, 2016

Household size	Port McNeill 2011	% of households	Port McNeill 2016	% of households
1 person	245	25%	295	29%
2 persons	375	38%	375	37%
3 persons	135	13%	160	16%
4 persons	160	16%	120	12%
5 or more persons	80	8%	60	6%
Total private households	995	100%	1010	100%
Total number of persons	2495	n/a	2330	n/a

Further details on household size are provided by the Census regarding the number of households relative to the number of persons living within. Within Port McNeill, the number of one (1) person households increased from 245 in 2011 to 295 in 2016, while the number of two (2) person stayed the same at 375 and three (3) person households increased (*Table 4*). In comparison, there was little change in RDMW's household sizes between the 2011 and 2016 Censuses (*Table 5*).

Table 5: RDMW Total Households by Household Size - 2011, 2016

Household size	RDMW 2011	% of households	RDMW 2016	% of households
1 person	1495	31%	1595	33%
2 persons	1780	37%	1805	37%
3 persons	680	14%	660	14%
4 persons	565	11%	480	10%
5 or more persons	355	7%	315	6%
Total private households	4875	100%	4855	100%
Total number of persons	11363	n/a	10870	n/a

Source: Statistics Canada, Census 2011-2016.

2.3.3 Average Households Income

Table 6: Average Household Income (after tax), 2016

Location	2016
Port McNeill	\$73,301
Regional District of Mount Waddington	\$28,672
British Columbia	\$61,280

Source: Statistics Canada, Census 2016.

Household income includes salaries, wages, retirement income, and government transfers of all persons residing within a household. Average household income in Port McNeill after-tax is 73,301 (*Table 6*). For context, the RDMW has an average household income (after tax) of 28,672.

2.4 IMPLICATIONS

There is a declining full-time population within Port McNeill, it is known there is a growing part-time population.

Although data from the Census and BC Stats was not available on the estimated transient resident population, it is assumed that there is not a large part-time or transient population.

Age characteristic data indicates that Port McNeill's population, like BC and Canada, continues to age. The design of public facilities and provision of public services will need to consider this demographic shift, including other daily services and amenities to help residents "age in place" and to support ongoing, independent living.

Household characteristic data indicates household size continues to decrease. The increase in the number of one (1) person households affirms the need to provide social and community services that prevents social isolation and continues to support a strong sense of community.



Housing Profile

3.1 DWELLINGS

The Census defines "dwellings" as meeting conditions for year-round occupancy. There are three major dwelling groups: 1. Occupied dwellings which are occupied by usual (year-round) residents, 2. Dwellings occupied by temporary residents, and 3. unoccupied dwellings.

3.1.1 Dwellings and Dwelling Occupancy

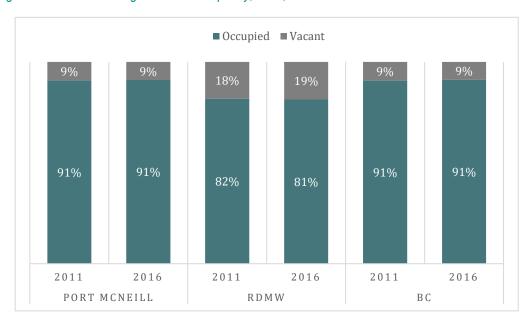
Total dwellings and occupied dwelling data for Port McNeill for the 2001-2016 Censuses are identified in *Table 7*. Occupancy rates for dwellings is derived between total dwellings and occupied dwellings. While the total number of dwellings has remained relatively constant, occupied dwellings have declined along with occupancy rates. Port McNeill's 2011 and 2016 occupancy rates are compared to RDMW and the province in *Figure 3*. Port McNeill and the province's occupancy rates are around 91% for both areas.

Table 7: Town of Port McNeill's Dwellings and Full-time Occupancy Rates, 2001-2016

Location	2001	2006	2011	2016
Total Dwellings	1134	1102	1106	1109
Total Occupied Dwellings	1075	1026	1005	1010
Full-time Occupancy Rate	95%	93%	91%	91%

Source: Statistics Canada, Census 2011-2016.

Figure 3: Percent Dwelling Full-time Occupancy, 2011, 2016



Source: Statistics Canada, Census 2016.

3.1.2 Housing Types

The majority (55%) of Port McNeill's dwellings are single-detached dwellings. Note, data between the 2011 and 2016 Censuses varies slightly for dwelling types. This is most likely the result of the same structures being reported differently in the two Census periods.

Table 8: Port McNeill Occupied Dwellings by Structural Type - 2011, 2016

Structural (Dwelling) type	2011	%	2016	%
Single-detached house	625	62%	550	55%
Apartment in a building that has five or more storeys	0	0%	0	0%
Apartment in a building that has fewer than five storeys	115	12%	100	10%
Row house	105	11%	120	12%
Semi-detached house	35	3%	30	3%
Apartment or flat in a duplex	0	0%	15	1%
Other single-attached house	0	0%	20	2%
Movable Dwelling	125	12%	175	17%
Total	1005	100%	1010	100%

Source: Statistics Canada, Census 2011-2016.

3.1.3 Age of Housing Stock

The majority (60%) of Port McNeill's housing was built prior to 1981. In comparison, 61% of the RDMW's housing stock was also built prior to 1981.

Table 9: Period of Construction - Percent (%) of Housing Stock, 2016

Time Period	Port McNeill	RDMW
1960 or before	4%	10%
1961-1980	56%	51%
1981-1990	19%	19%
1991-2000	18%	13%
2001-2005	1%	3%
2006-2010	1%	3%
2011-2016	1%	1%
Total	100%	100%

Source: Statistics Canada, Census 2016.

3.1.4 Housing Value

Housing value refers to the amount an owner expects if the asset is sold and includes land and all structures located on it for single-detached dwellings. For multiple dwellings within a structure (i.e., apartments, row house), it includes only the individual dwelling. Average values for dwellings are provided in *Table 10*. Also noted in the table, is the average value of dwellings from the 2006 Census.

Table 11: Housing Value - Average Value of Dwellings, 2006 and 2016

	Port McNeill 2006	Port McNeill 2016	RDMW 2016
Average Value of Dwellings	\$171,618	\$207,515	\$233,263

Source: Statistics Canada, Census 2006-2016.

3.2 HOUSING TENURE

Tenure refers to whether persons living within the dwelling own or rent it. In Port McNeill, 67% of dwellings are owner occupied. This is similar to the RDMW which is 68% of occupied dwellings (*Table 11*).

Table 10: Housing Tenure, 2016

Location	% Owner occupied	% renter occupied	% band housing
Port McNeill	67%	33%	0%
Regional District of Mount Waddington	68%	30%	2%

Source: Statistics Canada, Census 2016.

High ownership rates are also an indicator of housing affordability. Another housing indicator available from the Census is "housing suitability". In Port McNeill, 96.5% of housing is considered suitable for persons occupying the housing, meaning the dwelling has enough bedrooms for the size and composition of the household.

3.2.1 Household Maintainers

Household maintainers refers to the primary person, whether or not they are residing in the household, who pays the rent, mortgage, taxes, and utilities. In Port McNeill, the majority (60%) of household maintainers are between the ages of 55-74 (*Table 12*).

Table 12: Total Households by Age of Primary Household Maintainer, 2016

Age Group	Port McNeill	%	RDMW	%
15 to 24 years	45	5%	155	3%
25 to 34 years	165	16%	645	13%
35 to 44 years	200	20%	665	14%
45 to 54 years	215	21%	975	20%
55 to 64 years	185	18%	1220	25%
65 to 74 years	160	16%	845	17%
75 to 84 years	25	3%	310	7%
85 years and over	10	1%	45	1%
Total	1010	100%	4855	100%

Source: Statistics Canada, Census 2016.

3.3 IMPLICATIONS

As noted, the majority of housing is owner-occupied, single-detached dwellings built prior to 1981. While housing values have increased from the 2000s, the overall number suitable for year-round habitation has been maintained. The ability to "age in place" is another important consideration as the age of primary household maintainers continues to increase.

In light of COVID-19, It is possible that will be a trend toward working at home. This shift may result in a trend for more home offices and the desire for more flexible spaces as people are forced or choose to work from home.



Labour Force and Employment

4.1 EMPLOYMENT RATES

Labour force employment rates include all persons aged 15 and over who are participating in the labour force. In Port McNeill, there were 1395 residents participating in the labour force, 94% of which were employed (*Table 13*).

Of those that were employed, 37% worked full-time and 63% worked part year and/or part-time. The overall participation rate (relative to full-time residents) for Port McNeill is 74%. In comparison, RDMW's participation rate is 63% and the provincial participation rate is 64%. Participation rates are affected by a number of factors, including population age (retired vs. non-retired residents) and employment demand.

Historic participation rates are provided in *Table 14* for Port McNeill and BC. The participation rate is greater in Port McNeill than BC for 2001, 2006 and 2016.

Table 14: Labour Force Employment Rates, 2016

	Port McNeill	RDMW	ВС
Employed	94%	90%	93%
Unemployed	6%	10%	7%

Source: Statistics Canada, Census 2016.

Table 13: Port McNeill's Labour Force Participation Rates, 2001, 2006, 2016

	2001	2006	2016
Port McNeill	78.9%	81.6%	74.4%
ВС	65.2%	65.6%	64%

4.2 LABOUR FORCE BY INDUSTRY

Agriculture; forestry; fishing; and hunting, Retail trade, Construction, and Accommodation and food services are the largest classifications of workers in Port McNeill (*Table 15*).

Historic labour force population data is provided in *Table 16*. In 2001, agriculture and other resource-based industries was the largest labour force class (33%) followed by other services (20%).

Table 15: Labour Force Population by Industry – Port McNeill and RDMW, 2016

Industry	Port	%	RDMW	%
	McNeill			
Agriculture; forestry; fishing and hunting	360	26%	1005	18%
Mining; quarrying; and oil and gas extraction	15	1%	65	1%
Utilities	0	0%	15	0.5%
Construction	105	8%	400	7%
Manufacturing	50	4%	460	9%
Wholesale trade	25	2%	60	1%
Retail trade	180	13%	510	9%
Transportation and warehousing	60	4%	385	7%
Information and cultural industries	0	0%	40	1%
Finance and insurance	10	1%	75	1%
Rel estate and rental leasing	15	1%	50	1%
Professional; scientific and technical services	50	4%	185	3%
Management of companies and enterprises	0	0%	10	0.5%
Administrative and support; waste management and	35	2%	175	3%
remediation services				
Educational services	85	6%	395	7%
Health care and social assistance	115	8%	600	11%
Arts; entertainment and recreation	15	1%	105	2%
Accommodation and food services	110	8%	385	7%
Other services (except public administration)	60	4%	245	4%
Public administration	100	7%	360	7%
Total	1390	100%	5525	100%

Table 16: Port McNeill' Labour Force Population by Industry, 2001, 2006, 2016

Industry	2001	%	2006	%	2016	%
Agriculture and other resource-based industries	560	33%	435	26%	375	27%
Manufacturing and construction industries	230	14%	195	12%	155	12%
Wholesale and retail trade	195	11%	220	13%	265	19%
Finance and real estate	25	1%	25	2%	25	2%
Health and education		12%	220	12%	200	14%
Business services	145	9%	245	15%	310	4%
Other services	340	20%	330	20%	60	22%
Total	1705	100%	1670	100%	1390	100%

Source: Statistics Canada, Census 2001, 2006, 2016.

Note: Due to different categorization between the 2001, 2006, and 2016 census' some data had to be aggregated into similar categories. For example, construction and manufacturing were combined into one category 'manufacturing and construction industries.

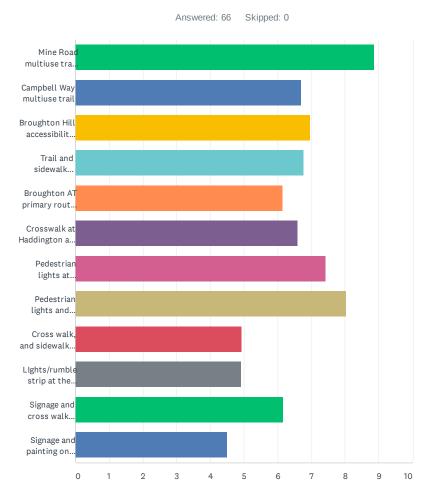
4.3 IMPLICATIONS

It is important to note that since 2001 Port McNeill's Agriculture and other resource-based industries has always been the greatest industry. Forestry has been the predominant industry in Port McNeill for several decades. The Data above has been aggregated. The Agriculture and other resource-based industries captures the forest industry statistics. The majority of jobs reported in this category represent forestry and forestry related positions.

Participation rates have reduced in this category, but it is still the most common. The reduced rate in Port McNeill's Agriculture and other resource-based industries is likely partially due to shift from a younger population with school age children to an older population that is retired and in need of different services and the younger generation not wanting to participate in this type of industry and moving out of Port McNeill in search of other work or education. The majority of this shift is likely the result of the change in the forestry industry on Vancouver Island.

Appendix 2
Consultation Data

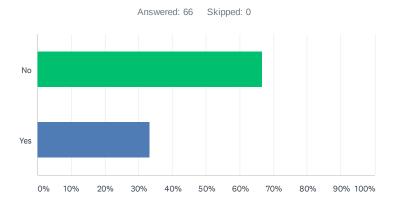
Q1 In your opinion, how do the following Active Transportation (AT) Capital Projects rank in importance? Please rank the following list of large, moderate, and low Capital Projects from most important to least important to you, 1 being most and 12 being least.



Prioritizing Active Transportation Projects in Port McNeill

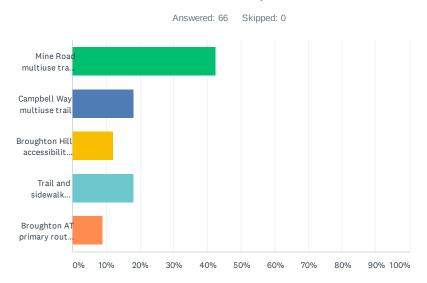
	1	2	3	4	5	6	7	8	9	10	11	12
Mine Road multiuse trail and sidewalk upgrades	30.30%	9.09%	13.64% 9	12.12% 8	7.58% 5	4.55% 3	1.52% 1	7.58% 5	3.03%	6.06% 4	3.03%	1.52% 1
Campbell Way multiuse trail	15.15% 10	12.12% 8	1.52% 1	6.06% 4	12.12% 8	4.55% 3	6.06% 4	4.55% 3	10.61% 7	6.06% 4	15.15% 10	6.06% 4
Broughton Hill accessibility sidewalk and multiuse trail with refuge areas	6.06%	15.15% 10	9.09%	9.09%	10.61% 7	4.55%	10.61% 7	9.09%	3.03%	9.09%	6.06%	7.58% 5
Trail and sidewalk connection from Beach Camp to Woodland on Mine Road	9.09%	13.64% 9	10.61% 7	9.09%	10.61%	6.06%	4.55% 3	1.52% 1	3.03%	7.58% 5	9.09%	15.15% 10
Broughton AT primary route infrastructure upgrades	7.58% 5	3.03%	6.06%	10.61%	9.09%	15.15% 10	0.00%	12.12%	13.64%	4.55%	9.09%	9.09%
Crosswalk at Haddington and Kingcome	3.03%	6.06%	6.06%	13.64% 9	7.58% 5	13.64% 9	16.67% 11	13.64% 9	4.55%	4.55%	4.55%	6.06%
Pedestrian lights at Haddington/Campbell Way	10.61% 7	10.61% 7	9.09%	6.06%	9.09%	10.61% 7	22.73% 15	4.55% 3	7.58% 5	3.03%	1.52%	4.55%
Pedestrian lights and crosswalk at Mine Road/Campbell Way	10.61% 7	16.67% 11	12.12% 8	9.09%	12.12% 8	10.61% 7	6.06%	12.12% 8	1.52%	1.52% 1	3.03%	4.55%
Cross walk, and sidewalk connecting Hemlock bus shelter to Campbell Way and McNeill Road	1.52% 1	1.52% 1	4.55%	6.06%	1.52% 1	4.55%	13.64% 9	13.64%	24.24% 16	13.64%	12.12% 8	3.03%
Lights/rumble strip at the slip lane at McNeill Road and Campbell Way	1.52%	3.03%	7.58% 5	6.06%	4.55%	7.58%	3.03%	6.06%	13.64%	30.30%	9.09%	7.58%
Signage and cross walk upgrade at Broughton and Campbell Way intersection	3.03%	4.55%	15.15% 10	7.58% 5	12.12% 8	7.58%	3.03%	9.09%	7.58% 5	10.61%	15.15% 10	4.55%
Signage and painting on primary, secondary, and tertiary routes	1.52% 1	4.55% 3	4.55% 3	4.55%	3.03%	10.61% 7	12.12% 8	6.06%	7.58% 5	3.03%	12.12% 8	30.30%

Q2 Is there another Capital Project that you feel is missing from the list of AT projects that should be included?



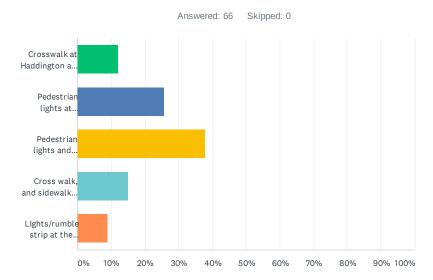
ANSWER CHOICES	RESPONSES	
No	66.67%	44
Yes	33.33%	22
TOTAL		66

Q3 Of the following large Capital Projects, choose one that you feel the Town of Port McNeill should complete before the others.



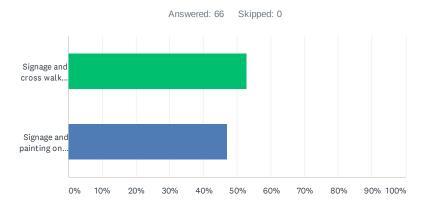
ANSWER CHOICES	RESPONSES	
Mine Road multiuse trail and sidewalk upgrades	42.42%	28
Campbell Way multiuse trail	18.18%	12
Broughton Hill accessibility sidewalk and multiuse trail with refuge areas	12.12%	8
Trail and sidewalk connection from Beach Camp to Woodland on Mine Road	18.18%	12
Broughton AT primary route infrastructure upgrades	9.09%	6
TOTAL		66

Q4 The following is a list of the moderate Capital Projects. Please choose the one that, in your opinion, should be completed before the others.



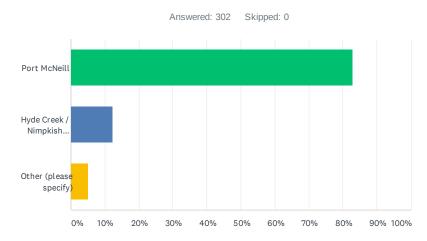
ANSWER CHOICES	RESPONS	ES
Crosswalk at Haddington and Kingcome	12.12%	8
Pedestrian lights at Haddington/Campbell Way	25.76%	17
Pedestrian lights and crosswalk at Mine Road/Campbell Way	37.88%	25
Cross walk, and sidewalk connecting Hemlock bus shelter to Campbell Way and McNeill Road	15.15%	10
Lights/rumble strip at the slip lane at McNeill Road and Campbell Way	9.09%	6
TOTAL		66

Q5 Of the low Capital Projects listed please choose the option that you would like the Town of Port McNeill to complete first.



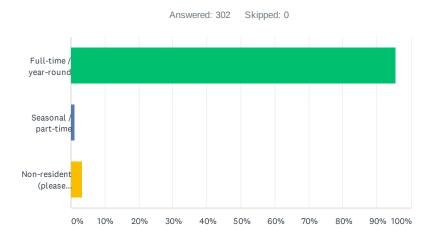
ANSWER CHOICES	RESPONSES	
Signage and cross walk upgrade at Broughton and Campbell Way intersection	53.03%	35
Signage and painting on primary, secondary, and tertiary routes	46.97%	31
TOTAL		66

Q1 Where do you reside?



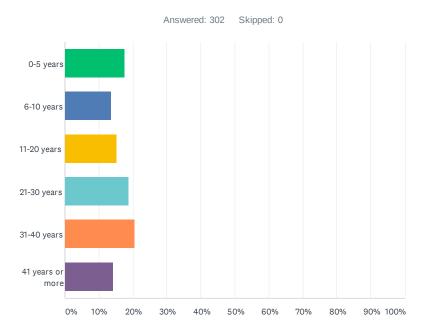
ANSWER CHOICES	RESPONSES	
Port McNeill	82.78%	250
Hyde Creek / Nimpkish Heights	12.25%	37
Other (please specify)	4.97%	15
TOTAL		302

Q2 What type of residency do you maintain in Port McNeill?



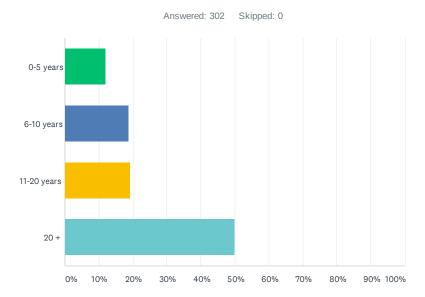
ANSWER CHOICES	RESPONSES	
Full-time / year-round	95.70%	289
Seasonal / part-time	0.99%	3
Non-resident (please specify)	3.31%	10
TOTAL		302

Q3 How long have you lived in Port McNeill, either as a part-time or full-time resident?



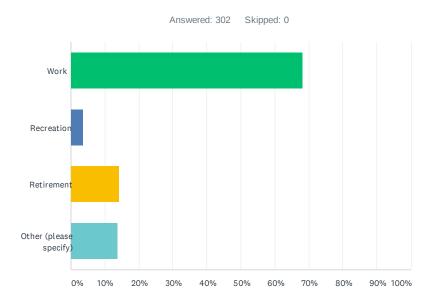
ANSWER CHOICES	RESPONSES	
0-5 years	17.55%	53
6-10 years	13.58%	41
11-20 years	15.23%	46
21-30 years	18.87%	57
31-40 years	20.53%	62
41 years or more	14.24%	43
TOTAL		302

Q4 How long do you see yourself residing in Port McNeill?



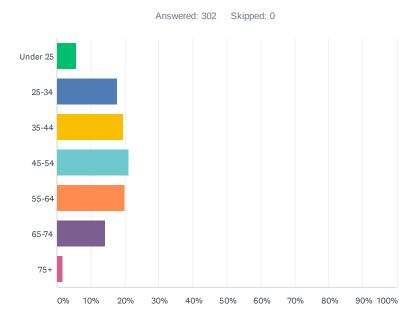
ANSWER CHOICES	RESPONSES	
0-5 years	11.92%	36
6-10 years	18.87%	57
11-20 years	19.21%	58
20 +	50.00%	151
TOTAL		302

Q5 What is the primary purpose of your full-time or part-time residency in Port McNeill?



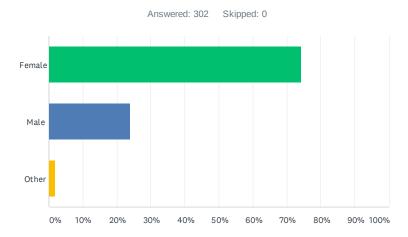
ANSWER CHOICES	RESPONSES	
Work	68.21%	206
Recreation	3.64%	11
Retirement	14.24%	43
Other (please specify)	13.91%	42
TOTAL		302

Q6 Please select your age group.



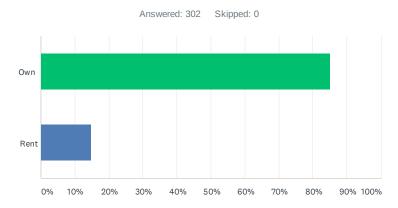
ANSWER CHOICES	RESPONSES	
Under 25	5.63%	17
25-34	17.88%	54
35-44	19.54%	59
45-54	21.19%	64
55-64	19.87%	60
65-74	14.24%	43
75+	1.66%	5
TOTAL		302

Q7 What is your gender?



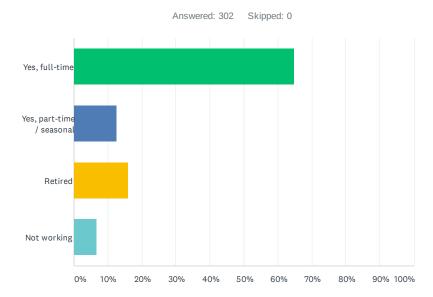
ANSWER CHOICES	RESPONSES
Female	74.17% 224
Male	23.84% 72
Other	1.99%
TOTAL	302

Q8 Do you own or rent where you live?



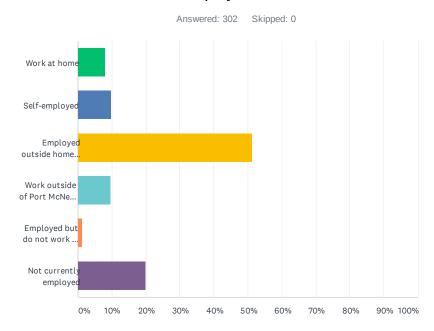
ANSWER CHOICES	RESPONSES	
Own	85.10%	257
Rent	14.90%	45
TOTAL		302

Q9 Are you employed?



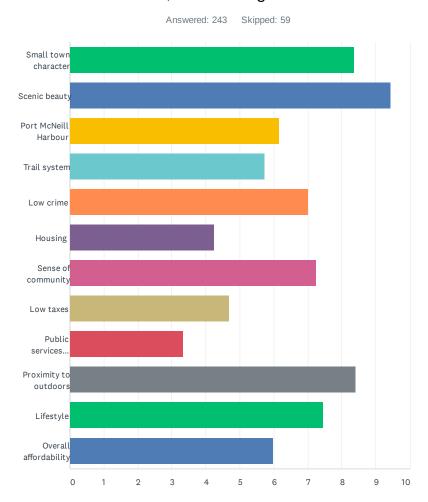
ANSWER CHOICES	RESPONSES	
Yes, full-time	64.90%	196
Yes, part-time / seasonal	12.58%	38
Retired	15.89%	48
Not working	6.62%	20
TOTAL		302

Q10 If employed, which of the following best describes your place of employment?



ANSWER CHOICES	RESPONSES	
Work at home	7.95%	24
Self-employed	9.93%	30
Employed outside home, but work in Port McNeill	51.32%	155
Work outside of Port McNeill on Vancouver Island	9.60%	29
Employed but do not work on Vancouver Island	1.32%	4
Not currently employed	19.87%	60
TOTAL		302

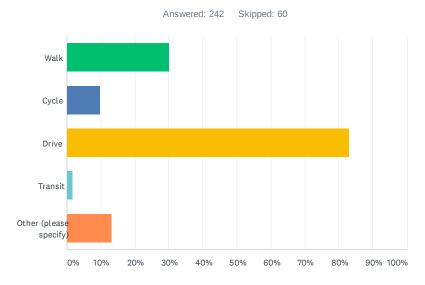
Q13 What do you feel are Port McNeill's major assets/amenities? Please rank from 1 to 12, with 1 being most and 12 least.



Port McNeill Official Community Plan and Zoning Bylaw Review Community Survey

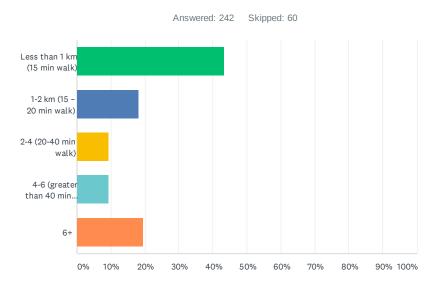
	1	2	3	4	5	6	7	8	9	10	11	12	TOTAL
Small town character	18.93% 46	15.23% 37	12.35% 30	8.23% 20	11.11% 27	6.17% 15	8.23% 20	5.76% 14	3.29% 8	4.12% 10	2.47% 6	4.12% 10	243
Scenic beauty	22.22% 54	25.93% 63	16.46% 40	7.82% 19	9.47% 23	4.94% 12	3.70% 9	0.82%	1.23%	2.88%	1.65% 4	2.88%	243
Port McNeill Harbour	3.29%	4.94% 12	7.41% 18	10.70% 26	9.88%	11.11% 27	7.41% 18	11.93% 29	9.05%	10.29% 25	9.47%	4.53% 11	243
Trail system	1.65% 4	3.29% 8	5.35% 13	9.05% 22	12.76% 31	10.70% 26	10.29% 25	10.70% 26	8.64% 21	8.23% 20	11.93% 29	7.41% 18	243
Low crime	5.35% 13	7.00% 17	7.82% 19	12.35% 30	12.76% 31	13.17% 32	9.47% 23	11.11% 27	8.23% 20	6.17% 15	4.53% 11	2.06% 5	243
Housing	1.65% 4	1.23%	5.35% 13	2.88%	3.29% 8	7.41% 18	7.41% 18	11.11% 27	10.29% 25	13.17% 32	17.28% 42	18.93% 46	243
Sense of community	8.23% 20	8.23% 20	9.05% 22	13.99% 34	9.05% 22	8.23% 20	14.81% 36	7.41% 18	8.64% 21	5.35% 13	4.12% 10	2.88%	243
Low taxes	0.82%	3.29% 8	2.88%	3.70%	7.00% 17	3.70%	10.70% 26	15.64% 38	12.35% 30	17.70% 43	11.93% 29	10.29% 25	243
Public services (including recreation facility, library)	1.23%	2.06%	1.65%	0.82%	0.82%	5.35% 13	4.94% 12	5.76% 14	14.40% 35	15.23% 37	17.28% 42	30.45% 74	243
Proximity to outdoors	21.40% 52	10.70% 26	13.58% 33	11.11% 27	9.05% 22	7.82% 19	5.35% 13	4.94% 12	4.53% 11	6.17% 15	3.29% 8	2.06%	243
Lifestyle	9.47% 23	11.93% 29	10.70% 26	11.52% 28	6.58% 16	12.76% 31	7.82% 19	7.82% 19	7.41% 18	3.70%	9.05%	1.23%	243
Overall affordability	5.76% 14	6.17% 15	7.41% 18	7.82% 19	8.23% 20	8.64% 21	9.88%	7.00%	11.93% 29	7.00%	7.00% 17	13.17% 32	243

Q16 How do you commute to and from work (please mark all that apply):



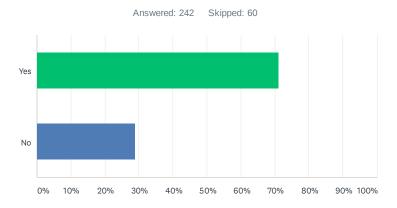
ANSWER CHOICES	RESPONSES	
Walk	30.17%	73
Cycle	9.92%	24
Drive	83.06%	201
Transit	1.65%	4
Other (please specify)	13.22%	32
Total Respondents: 242		

Q17 How far is your typical commute?



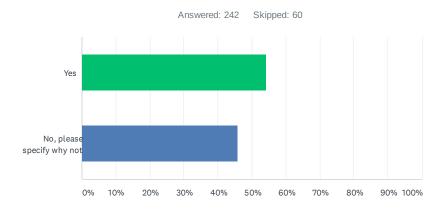
ANSWER CHOICES	RESPONSES	
Less than 1 km (15 min walk)	43.39%	105
1-2 km (15 – 20 min walk)	18.18%	44
2-4 (20-40 min walk)	9.50%	23
4-6 (greater than 40 min walk)	9.50%	23
6+	19.42%	47
TOTAL		242

Q18 Do you have a safe way to walk or bike to work?



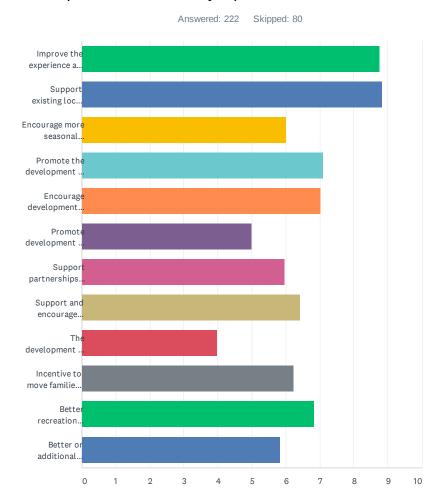
ANSWER CHOICES	RESPONSES	
Yes	71.07%	172
No	28.93%	70
TOTAL		242

Q19 Would you cycle or walk to work if there was a safe and accessible trail, sidewalk or bike lane to your place of work?



ANSWER CHOICES	RESPONSES	
Yes	54.13%	131
No, please specify why not	45.87%	111
TOTAL		242

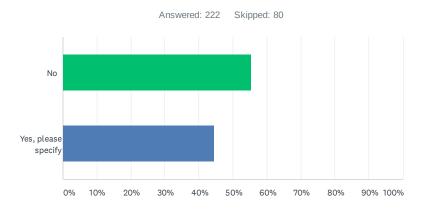
Q20 What do you consider the single most important investment to help develop the local economy? (rank from 1 most to 12 least)



Port McNeill Official Community Plan and Zoning Bylaw Review Community Survey

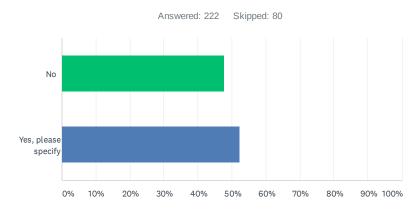
	1	2	3	4	5	6	7	8	9	10	11	12	Т
Improve the experience and aesthetic of the downtown	29.73% 66	15.77% 35	8.11% 18	6.31% 14	8.56% 19	6.76% 15	6.31% 14	2.70%	4.50% 10	4.05%	3.60%	3.60%	
Support existing local businesses (i.e. promote 'shop local')	18.47% 41	17.57% 39	13.96% 31	11.71% 26	9.01%	7.21% 16	8.11% 18	4.50% 10	4.50% 10	1.80%	1.35%	1.80%	
Encourage more seasonal businesses	0.90%	6.31% 14	11.71% 26	7.66% 17	6.76% 15	8.11% 18	7.66% 17	16.22% 36	11.71% 26	9.46%	8.11% 18	5.41% 12	
Promote the development of tourist-related services, including eco-tourism	5.41% 12	6.76% 15	7.21% 16	18.47% 41	13.96% 31	7.21% 16	13.51% 30	4.95% 11	9.46% 21	4.05%	5.41% 12	3.60%	
Encourage development along the waterfront	5.41% 12	11.26% 25	11.71% 26	8.56% 19	14.86% 33	9.01% 20	8.56% 19	6.31% 14	4.50% 10	4.50% 10	7.21% 16	8.11% 18	
Promote development of home-based businesses	1.35%	3.15%	5.41% 12	4.95% 11	7.66% 17	11.71% 26	9.01%	6.31% 14	13.51%	9.01%	13.96% 31	13.96% 31	
Support partnerships with major employers in locating to Port McNeill	6.31% 14	6.31% 14	3.60%	5.41% 12	8.56% 19	10.81% 24	12.61% 28	11.71% 26	7.21% 16	12.61% 28	7.21% 16	7.66% 17	
Support and encourage emerging industries	7.21% 16	7.21% 16	8.11% 18	5.41% 12	7.21% 16	10.36% 23	9.01%	14.86% 33	9.01%	11.26% 25	8.56% 19	1.80%	
The development of privately managed forests (have taxable active uses on the properties)	1.35%	2.70%	2.70%	2.70%	4.05% 9	6.31%	4.50% 10	9.46% 21	12.61% 28	13.96%	17.12% 38	22.52% 50	
Incentive to move families – area to develop houses	8.56% 19	7.66% 17	4.95% 11	9.91% 22	7.21% 16	9.01% 20	6.31% 14	8.56% 19	7.21% 16	12.61% 28	10.81% 24	7.21% 16	
Better recreation services	11.71% 26	6.31% 14	13.06% 29	10.81% 24	7.21% 16	4.50% 10	7.21% 16	7.66% 17	7.21% 16	5.41% 12	12.16% 27	6.76% 15	
Better or additional infrastructure	3.60%	9.01%	9.46%	8.11% 18	4.95% 11	9.01%	7.21% 16	6.76%	8.56% 19	11.26% 25	4.50% 10	17.57% 39	

Q21 Are there other economic opportunities in Port McNeill that you feel should be targeted?



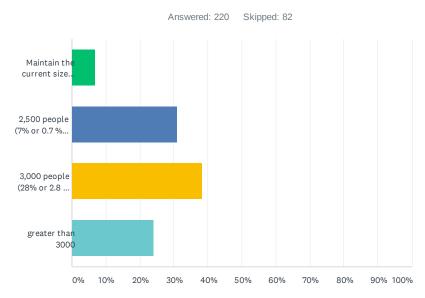
ANSWER CHOICES	RESPONSES	
No	55.41%	123
Yes, please specify	44.59%	99
TOTAL		222

Q22 Do you feel there are assets/amenities that go unleveraged or could be promoted to facilitate greater Economic Development?



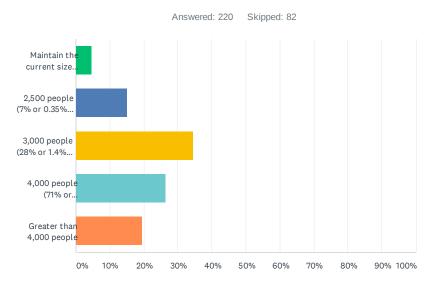
ANSWER CHOICES	RESPONSES	
No	47.75%	106
Yes, please specify	52.25%	116
TOTAL		222

Q23 As the Town continues to grow, what population would you like to see Port McNeill grow to over the next 10 years?



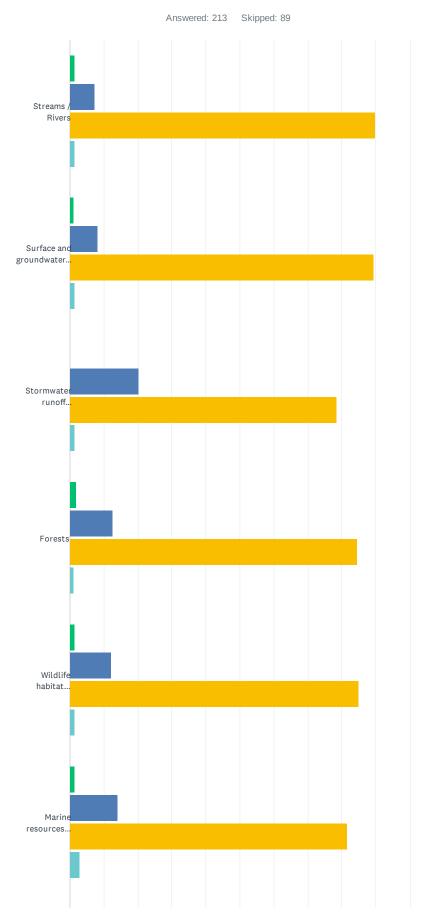
ANSWER CHOICES	RESPONSES	
Maintain the current size with no growth (currently approximately 2,337 people)	6.82%	15
2,500 people (7% or 0.7 %/ year = 16 people / year)	30.91%	68
3,000 people (28% or 2.8 % / year = 65 people/ year)	38.18%	84
greater than 3000	24.09%	53
TOTAL		220

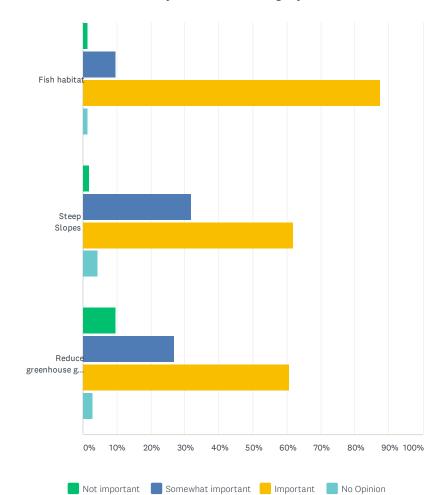
Q25 How many people would you like to see ultimately living in Port McNeill 20 years from now?



ANSWER CHOICES	RESPONSES	S
Maintain the current size with no growth (currently approximately 2,337 people)	4.55%	10
2,500 people (7% or 0.35% year = 8 people/ year)	15.00%	33
3,000 people (28% or 1.4% / year = 33 people/ year)	34.55%	76
4,000 people (71% or 3.5%/year = 82 people/year)	26.36%	58
Greater than 4,000 people	19.55%	43
TOTAL		220

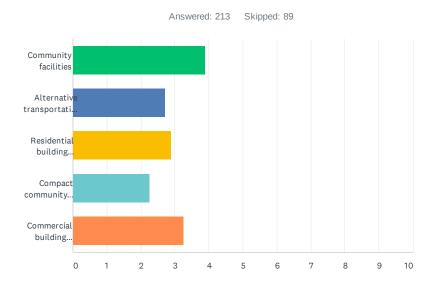
Q27 How important is it to achieve/protect the following?





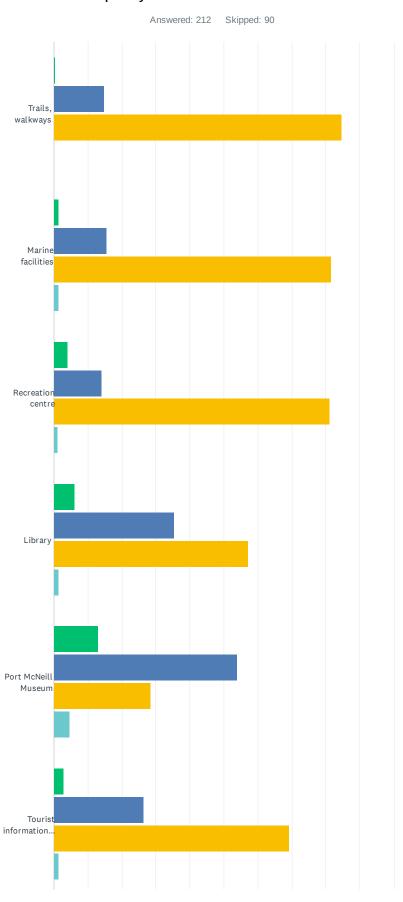
	NOT IMPORTANT	SOMEWHAT IMPORTANT	IMPORTANT	NO OPINION	TOTAL
Streams / Rivers	1.46% 3	7.28% 15	89.81% 185	1.46% 3	206
Surface and groundwater quality	0.96%	8.17% 17	89.42% 186	1.44%	208
Stormwater runoff management	0.00%	20.19%	78.37% 163	1.44%	208
Forests	1.94% 4	12.62% 26	84.47% 174	0.97%	206
Wildlife habitat management	1.46% 3	12.14% 25	84.95% 175	1.46%	206
Marine resources (i.e., marina and harbour)	1.46% 3	14.08% 29	81.55% 168	2.91%	206
Fish habitat	1.46%	9.71% 20	87.38% 180	1.46%	206
Steep Slopes	1.93%	31.88% 66	61.84% 128	4.35% 9	207
Reduce greenhouse gas emissions	9.71% 20	26.70% 55	60.68% 125	2.91% 6	206

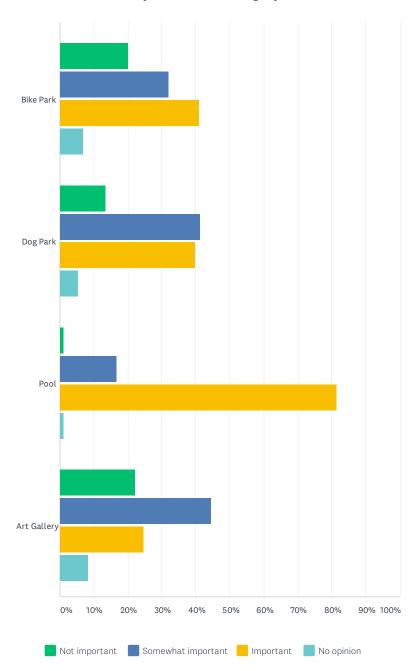
Q29 The Town has committed to Green House Gases (GHG) emissions reductions. Which areas should the town focus on to meet the community's GHG reduction targets (prioritize 1 most important – 5 least important)?



	1	2	3	4	5	TOTAL	SCORE
Community facilities	40.38%	26.29%	19.72%	8.92%	4.69%		
	86	56	42	19	10	213	3.89
Alternative transportation options	15.02%	16.43%	16.43%	28.64%	23.47%		
	32	35	35	61	50	213	2.71
Residential building efficiency	12.68%	20.19%	26.76%	23.47%	16.90%		
	27	43	57	50	36	213	2.88
Compact community design	9.39%	9.39%	15.96%	28.64%	36.62%		
	20	20	34	61	78	213	2.26
Commercial building efficiency	22.54%	27.70%	21.13%	10.33%	18.31%		
	48	59	45	22	39	213	3.26

Q30 How important are the following services and amenities in supporting quality of life in Port McNeill?

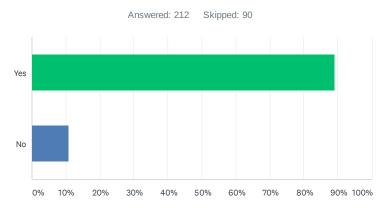




Port McNeill Official Community Plan and Zoning Bylaw Review Community Survey

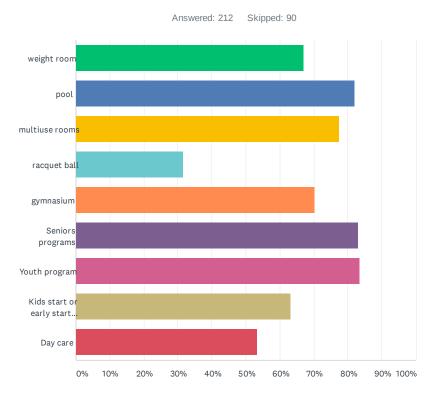
	NOT IMPORTANT	SOMEWHAT IMPORTANT	IMPORTANT	NO OPINION	TOTAL	WEIGHTED AVERAGE
Trails, walkways	0.50% 1	14.85% 30	84.65% 171	0.00%	202	2.84
Marine facilities	1.49% 3	15.42% 31	81.59% 164	1.49%	201	2.83
Recreation centre	3.98%	13.93% 28	81.09% 163	1.00%	201	2.79
Library	5.97% 12	35.32% 71	57.21% 115	1.49%	201	2.54
Port McNeill Museum	13.00% 26	54.00% 108	28.50% 57	4.50% 9	200	2.25
Tourist information centre	3.03%	26.26% 52	69.19% 137	1.52%	198	2.69
Bike Park	20.00%	32.00% 64	41.00% 82	7.00% 14	200	2.35
Dog Park	13.43% 27	41.29% 83	39.80% 80	5.47% 11	201	2.37
Pool	0.99%	16.75% 34	81.28% 165	0.99%	203	2.82
Art Gallery	22.28% 45	44.55% 90	24.75% 50	8.42% 17	202	2.19

Q31 Do you support the development of a Port McNeill plaza/community square/community gathering area?



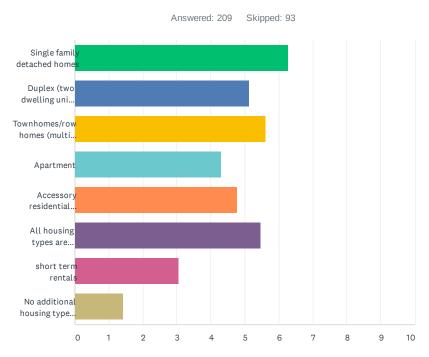
ANSWER CHOICES	RESPONSES	
Yes	89.15%	189
No	10.85%	23
TOTAL		212

Q32 What facilities or programs do you envision in a Recreation Centre (check all that apply):



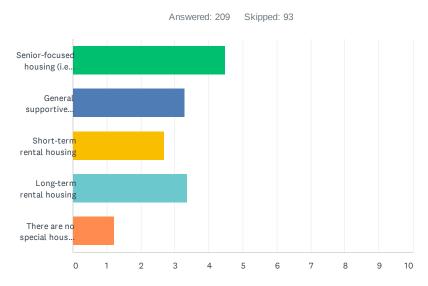
ANSWER CHOICES	RESPONSES	
weight room	66.98%	142
pool	82.08%	174
multiuse rooms	77.36%	164
racquet ball	31.60%	67
gymnasium	70.28%	149
Seniors programs	83.02%	176
Youth program	83.49%	177
Kids start or early start programs	63.21%	134
Day care	53.30%	113
Total Respondents: 212		

Q33 In your opinion, what additional type of housing is most needed in Port McNeill (prioritize from 1 highest to 8 lowest)?



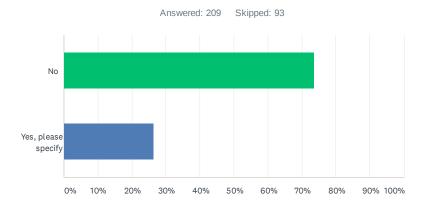
	1	2	3	4	5	6	7	8	TOTAL	SCORE
Single family detached homes	26.79% 56	30.14% 63	14.35% 30	10.05% 21	10.53% 22	6.70% 14	1.44% 3	0.00%	209	6.27
Duplex (two dwelling units in one building)	2.87% 6	13.40% 28	26.79% 56	26.79% 56	14.83% 31	11.00% 23	4.31% 9	0.00%	209	5.12
Townhomes/row homes (multiple dwelling units in one building with front door access from a street)	11.48% 24	19.14% 40	22.01% 46	24.88% 52	13.40% 28	6.22%	2.39%	0.48%	209	5.60
Apartment	1.91%	11.00% 23	11.96% 25	16.27% 34	24.88% 52	19.14% 40	12.44% 26	2.39% 5	209	4.30
Accessory residential (i.e., secondary suites, granny flats, tiny homes)	6.70% 14	16.75% 35	16.27% 34	11.96% 25	15.79% 33	20.57% 43	11.00% 23	0.96%	209	4.76
All housing types are needed	46.41% 97	3.83%	1.44%	4.31% 9	9.09% 19	17.22% 36	13.40% 28	4.31% 9	209	5.47
short term rentals	1.44%	5.26% 11	5.74% 12	5.26% 11	10.05% 21	17.22% 36	49.28% 103	5.74% 12	209	3.05
No additional housing types are needed	2.39%	0.48%	1.44%	0.48%	1.44%	1.91%	5.74% 12	86.12% 180	209	1.43

Q34 In your opinion, what type of special needs housing is most needed (prioritize from 1 highest to 5 lowest)?



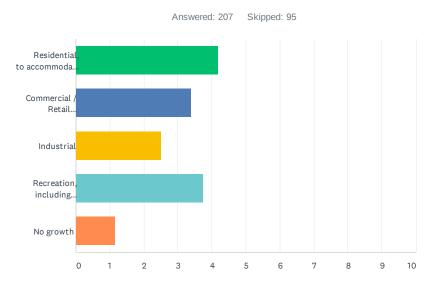
	1	2	3	4	5	TOTAL	SCORE
Senior-focused housing (i.e., support services, assisted-care)	65.55% 137	21.05% 44	9.57% 20	2.87% 6	0.96%	209	4.47
General supportive housing (provides life-skills services)	10.05% 21	37.32% 78	26.79% 56	22.01% 46	3.83%	209	3.28
Short-term rental housing	3.83%	15.31% 32	30.62% 64	45.93% 96	4.31% 9	209	2.68
Long-term rental housing	18.18% 38	25.84% 54	31.10% 65	23.44% 49	1.44%	209	3.36
There are no special housing needs in Port McNeill	2.39%	0.48%	1.91%	5.74% 12	89.47% 187	209	1.21

Q35 In your opinion, are there other types of additional housing needed?



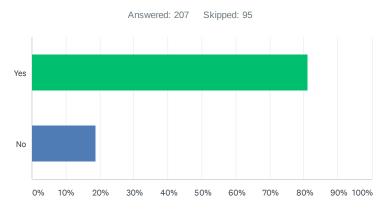
ANSWER CHOICES	RESPONSES	
No	73.68%	154
Yes, please specify	26.32%	55
TOTAL		209

Q36 What should be the highest priority for future growth and development (rank 1 highest priority to 5 lowest)?



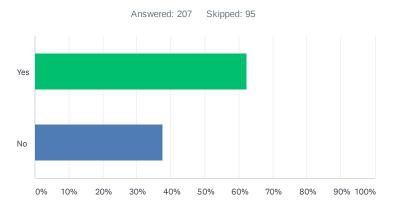
	1	2	3	4	5	TOTAL	SCORE
Residential, to accommodate seasonal and full-time residents	53.14% 110	21.26% 44	17.87% 37	7.25% 15	0.48%	207	4.19
Commercial / Retail including downtown	10.14% 21	35.75% 74	38.16% 79	14.01% 29	1.93% 4	207	3.38
Industrial	5.31% 11	9.66% 20	19.32% 40	62.32% 129	3.38%	207	2.51
Recreation, including marine facilities, park space	28.99% 60	32.85% 68	23.67% 49	13.53% 28	0.97%	207	3.75
No growth	2.42%	0.48%	0.97%	2.90%	93.24% 193	207	1.16

Q38 Do you believe vacation rentals such as Air BnBs and short term rentals should be permitted in Town?



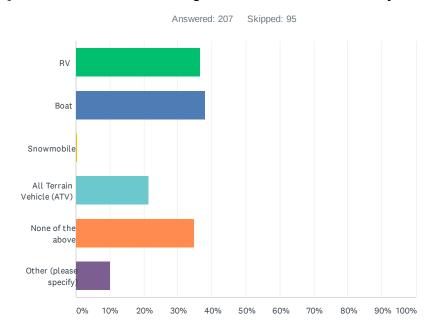
ANSWER CHOICES	RESPONSES	
Yes	81.16%	168
No	18.84%	39
TOTAL		207

Q40 Do you own a recreational vehicle?



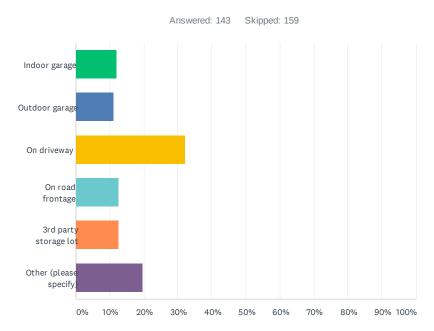
ANSWER CHOICES	RESPONSES	
Yes	62.32%	129
No	37.68%	78
TOTAL		207

Q41 Which of the following recreational vehicles do you own?



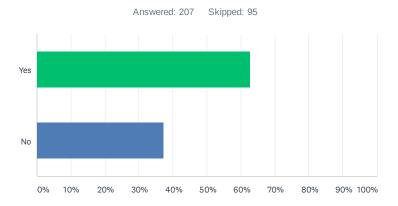
ANSWER CHOICES	RESPONSES	
RV	36.71%	76
Boat	38.16%	79
Snowmobile	0.48%	1
All Terrain Vehicle (ATV)	21.26%	44
None of the above	34.78%	72
Other (please specify)	10.14%	21
Total Respondents: 207		

Q42 Where do you store your recreational vehicle(s) when they are not in use? Please skip to the next question if you do not own a recreational vehicle.



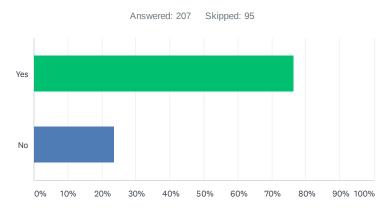
ANSWER CHOICES	RESPONSES
Indoor garage	11.89% 17
Outdoor garage	11.19%
On driveway	32.17% 46
On road frontage	12.59% 18
3rd party storage lot	12.59% 18
Other (please specify)	19.58% 28
TOTAL	143

Q43 Would you support a ban on street parking of recreation vehicles?



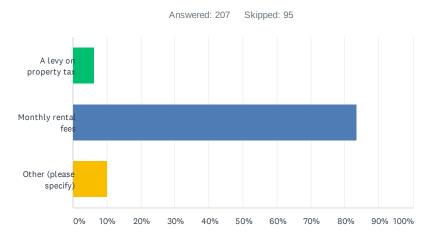
ANSWER CHOICES	RESPONSES	
Yes	62.80%	130
No	37.20%	77
TOTAL		207

Q44 Would you support a community owned storage site for recreational vehicles?



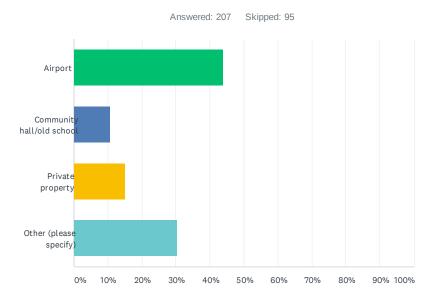
ANSWER CHOICES	RESPONSES	
Yes	76.33%	158
No	23.67%	49
TOTAL		207

Q45 What method of revenue generation would be acceptable to you to fund the operation of the community storage site?



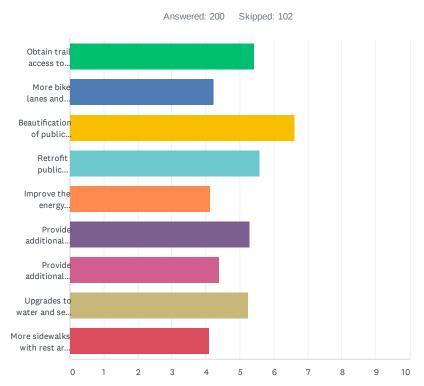
ANSWER CHOICES	RESPONSES	
A levy on property tax	6.28%	13
Monthly rental fees	83.57%	173
Other (please specify)	10.14%	21
TOTAL		207

Q46 What area of Port McNeill should it be located?



ANSWER CHOICES	RESPONSES	
Airport	43.96%	91
Community hall/old school	10.63%	22
Private property	14.98%	31
Other (please specify)	30.43%	63
TOTAL		207

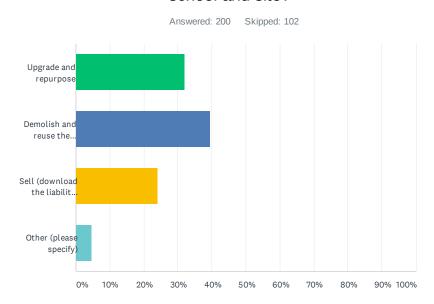
Q47 Considering the finite resources of the Town, how would you, at a high-level, prioritize the following emerging issues facing Port McNeill? Please rank from 1 highest priority to 9 lowest priority.



Port McNeill Official Community Plan and Zoning Bylaw Review Community Survey

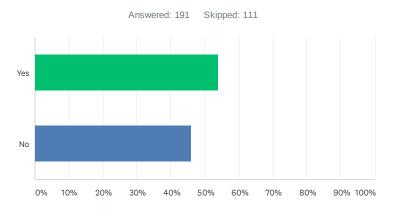
	1	2	3	4	5	6	7	8	9	TOTAL	SCORE
Obtain trail access to recreation areas.	12.50% 25	17.00% 34	13.50% 27	8.50% 17	9.00% 18	9.50% 19	12.00% 24	11.00% 22	7.00% 14	200	5.42
More bike lanes and trails connecting in town destinations	3.50% 7	12.50% 25	8.00% 16	9.50%	14.50% 29	4.50% 9	10.50%	19.50% 39	17.50% 35	200	4.23
Beautification of public spaces (Downtown)	35.50% 71	13.50% 27	9.00% 18	10.00%	7.50% 15	11.50% 23	5.50% 11	2.50%	5.00%	200	6.61
Retrofit public facilities to better accommodate aging in place	7.50% 15	14.00% 28	16.00% 32	18.00% 36	14.50% 29	11.00% 22	9.00%	6.50% 13	3.50% 7	200	5.59
Improve the energy efficiency of public buildings to meet greenhouse gas reduction targets.	3.50%	6.50%	9.50%	7.50% 15	13.00% 26	18.50% 37	12.00% 24	12.00% 24	17.50% 35	200	4.12
Provide additional recreation centre programs to accommodate aging in place.	9.50%	10.00%	10.50%	17.50% 35	11.50%	17.50% 35	13.50% 27	6.50%	3.50% 7	200	5.29
Provide additional marine facilities (wharves, docks) to accommodate the growing number of seasonal visitors.	5.50%	9.00%	13.00% 26	10.50%	7.50% 15	10.50%	12.50% 25	15.50% 31	16.00% 32	200	4.39
Upgrades to water and sewer to accommodate residential development	17.50% 35	10.50%	7.50% 15	12.00% 24	11.50% 23	7.00% 14	12.50% 25	14.00% 28	7.50% 15	200	5.25
More sidewalks with rest areas for seniors	5.00%	7.00% 14	13.00% 26	6.50% 13	11.00% 22	10.00%	12.50% 25	12.50% 25	22.50% 45	200	4.11

Q49 The Town owns and operates the old school site. Currently it is costing the town \$90,000 / year to operate and maintain the exiting building. It has been estimated that the old school will cost \$1 million to upgrade or to demolish. What would you prefer to be done with the old school and site?



ANSWER CHOICES	RESPONSES	
Upgrade and repurpose	32.00%	64
Demolish and reuse the property	39.50%	79
Sell (download the liability to the private sector to repurpose the property)	24.00%	48
Other (please specify)	4.50%	9
TOTAL		200

Q54 Would you like us to send you updates on the OCP and Zoning Bylaw process?



ANSWER CHOICES	RESPONSES	
Yes	53.93%	103
No	46.07%	88
TOTAL		191

Appendix 3

Data Collection

1. Data Collection

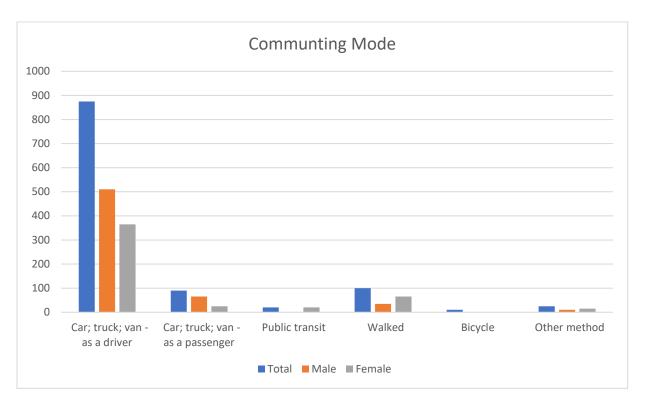
1.1. CURRENT STATE OF ACTIVE TRANSPORTATION IN PORT MCNEILL TODAY

The Census data from 2016 was used to assess the current state of mode split and to assess the average commuting times for local commuters. In addition, utilization and desired to use at data was collected through community survey. The census summary and finding are presented below:

2016 Census Data

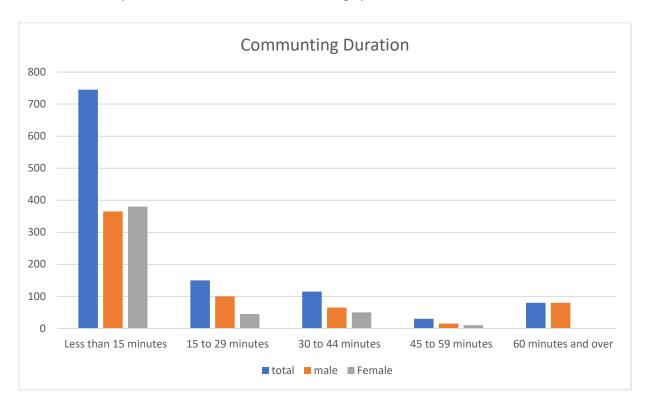
1.1.1.Mode

The data shows that the approval 9.8% of residents in Port McNeill community using an active mode of transportation, predominantly walking or cycling. These statistics were supported by the data collected in the community survey (see Survey data section below).



Transportation Mode	Total	Male	Female
Car; truck; van - as a driver	875	510	365
Car; truck; van - as a passenger	90	65	25
Public transit	20	0	20
Walked	100	35	65
Bicycle	10	0	0
Other method	25	10	15

A 10 % mode share is a reasonable mode share, however it is anticipated with dedicated Active transportation infrastructure development that this mode share can easily be increased due to the proximity of existing residential uses to key employment and shopping destinations within the community. This proximity is shown on the destination map section and illustrated the in table and graph below.



Commuting Duration	total	male	Female
Less than 15 minutes	745	365	380
15 to 29 minutes	150	100	45
30 to 44 minutes	115	65	50
45 to 59 minutes	30	15	10
60 minutes and over	80	80	0

Over 66% of all commutes in Port McNeill are under 15 minutes in length. This commute length is ideal for alternative modes of transportation. The majority of residents commuting under 15 minutes will be working within close proximity to town. It is not possible to transition all of the less than 15 minute communities to active transportation as the largest employers in the area are resource based industries that often require specialised equipment, however a significant portion could be encouraged and incentivized to transition to active modes of transportation. The primary way to foster mode choice transition to active modes is by removing key barriers.

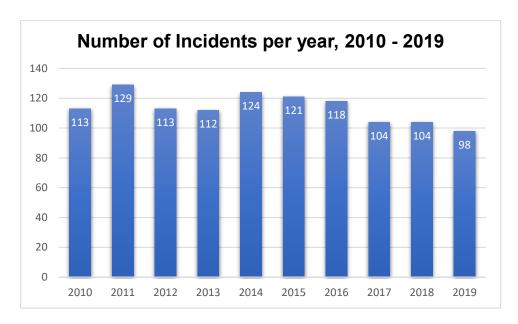
Source: Census Data 2016

1.2. CRASH SITES AND SAFETY CONCERNS

ICBC data was reviewed to determine the areas of high conflict. This information was considered in combination with the feedback from the public consultation process to identify key areas of safety concern. Each of these areas have been identified as a one of the active transportation projects as listed in the Implementation section in the report.

Data Analysis of ICBC Incidents

Between 2010 and 2019 there were a total of 1,136 incidents.



The Incidents took place at 151 different Street Locations in Port McNeill with the highest number over the years 2010 to 2019 at the following locations:

- Hwy 19 had 336 incidents (30%)
- Campbell Way had 86 incidents (7.5%)
- Island Hwy had 54 incidents (4.7%)

The highest number of incidents to include a recorded cross street were at Campbell Way (40 occurrences).

Out of the 1,136 ICBC recorded incidents, one involved a pedestrian and two involved a bicycle.

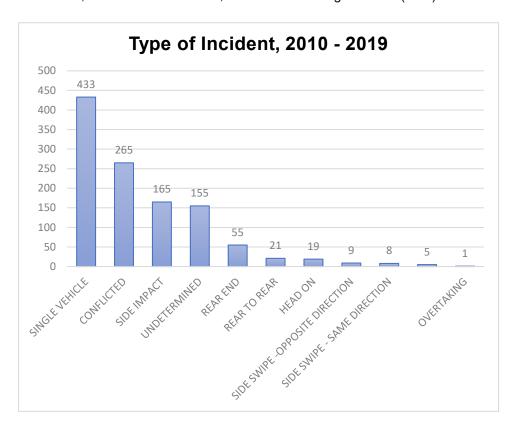
The pedestrian incident was located on Catala Place

The Cycling incidents were located on:

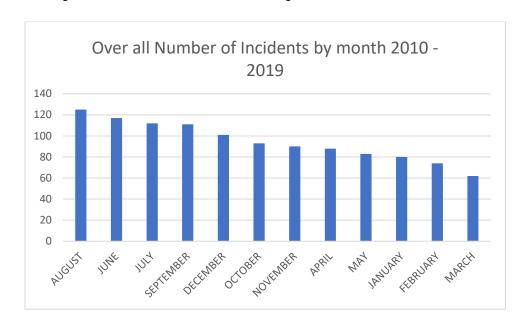
- Campbell Way and Hemlock Street
- Haddington Crescent and Kingcome Place

*The Interactive Engagement Map indicated three people would like to see a bike path along Haddington. A comment was also received about a blind curve and a request for a 30km speed sign prior to the curve on Haddington Crescent.

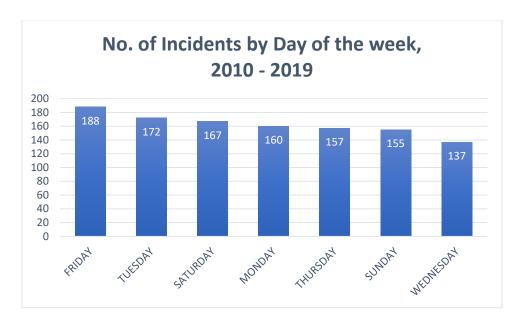
Of the total 1,136 recorded incidents, 433 involved a single vehicle (38%).



The highest number of incidents occur in August and the least in March over the ten-year period.



The highest number of incidents over the ten-year period occurred on Fridays.



Out of the 1136 incidents, 259 occur between 3 to 6 p.m. This information informed the assessment and review of potential improvement projects listed in Section 5 of the AT plan.

1.3. GHG EMISSIONS REDUCTIONS

The current active transportation mode share in Port McNeil is reported at 10% or \sim 230 people. The goal is to have 15% more share by 2030. For calculations of GHG reductions it is assumed that the current active mode share is 10% \sim 230 individuals. Based on the goals and assumptions, an increase of 120 individuals will utilize active modes by 2030.

For the calculation we have assumed the following based on 2014 BC Best Practices Methodology For Quantifying Greenhouse Gas Emissions Including Guidance For Public Sector Organizations, Local Governments And Community Emissions. We have used the Light Duty Truck vehicle emission in the following calculations:

Average vehicle distance traveled	15,000km/year
Light duty truck vehicle	2.353 Kg/L COe (source MOE)
Average fuel economy	8.9 L/100 km source (Canada Energy Regulator, accessed on Jan 14, 2020)

Sample Calculation:

Fuel Consumption: 15,000km/year / 100km X 8.9L = 1,335 L/year

GHG emissions/ Vehicle: 1,335 L/year X 2.353 Kg/L = 3,141.255 Kg/year

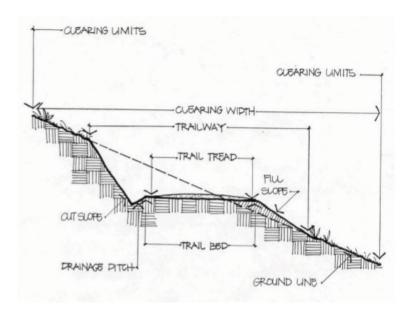
This provides an estimate for the COe / vehicle/ year. This is used as the basis for the calculation of potential COe reductions. It is assumed that due to the climate and weather in Port McNeill most people will use a vehicle to commute during the winter and rainy season. The following table summarizes to potential COe reduction based on the four scenarios for Active Transportation commuter's active mode utilization levels.

% of Trips	Tonnes of COe/year/vehicle	Total potential reduction (tonnes) per year
100% shift scenario –	3.1	372
75% shift Scenario	2.3	276
50% Shift scenario -	1.55	186
25% shift scenario	0.78	93.6

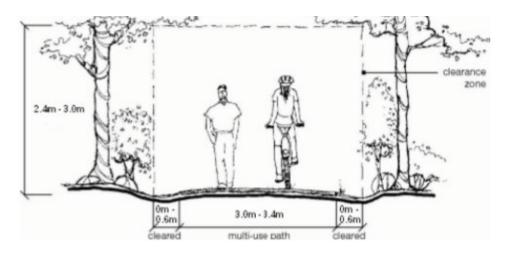
Based on the calculations above, the likely GHG reduction from an increase in active mode share will result a 186 tonne reduction per year.

Appendix 4 Standards

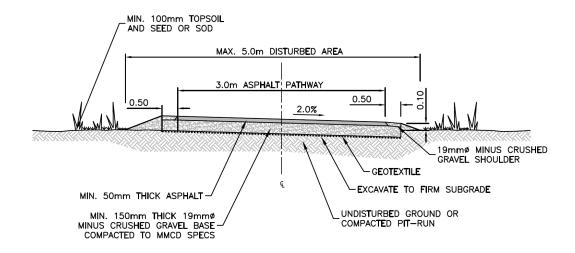
TYPICAL CROSS SECTIONS FOR TRAIL IMPLEMENTATION



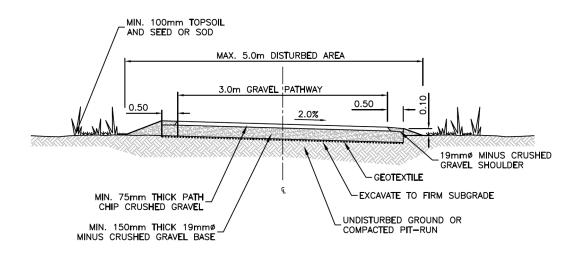
Trail located on a slope. City of Whitehorse Trail Plan



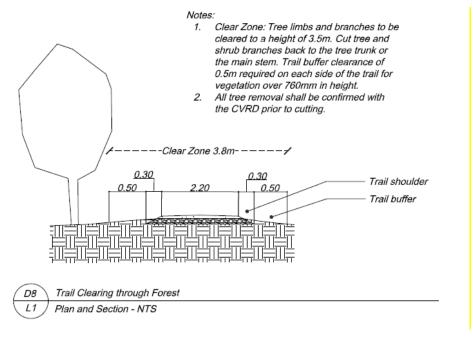
Multi-use trail. City of Fredericton Trails/Bikeways Master Plan



Type I - Multi-use/Bike - Path Packed Screens. CVRD Trail specifications



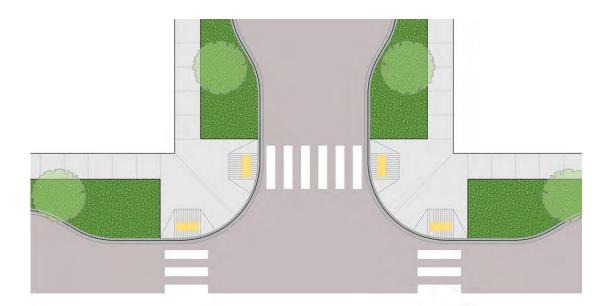
Type II - Walking Paths. CVRD Trail specifications



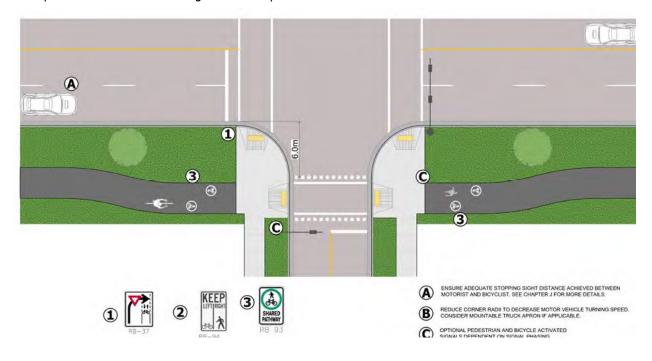
Type III – Mountain Biking/Hiking Paths. CVRD Trails Master Plan?

CROSSINGS

Recommended crossing in residential neighbourhoods



Example of recommended crossing of multi-use path on residential street.



Buffer space options for Bike lanes adjacent to parking (Figure D-48)



Appendix 5

Cost Estimates

Date:

2021-03-08



Project: 2221-49171-2019

Town of Port McNeill Class C Construction Cost Estimate Multi-use Pathway on Mine Rd

Multi-Use Pathway on Mine Rd	\$	1,630,903
Pedestrian Activated Crosswalk (Haddington Cr and Campbell Way)	\$	30,000
Pedestrian Activated Crosswalk (Mine Rd and Campbell Way)	\$	30,000
Sub-Total Sub-Total	\$	1,690,903
General Contingency Allowance(30%) \$	507,271
Allowance for Engineering, Legal, Construction, Financial and Administration Costs (15%) \$	253,635
Total	\$	2,452,000

2021-03-08

Date:

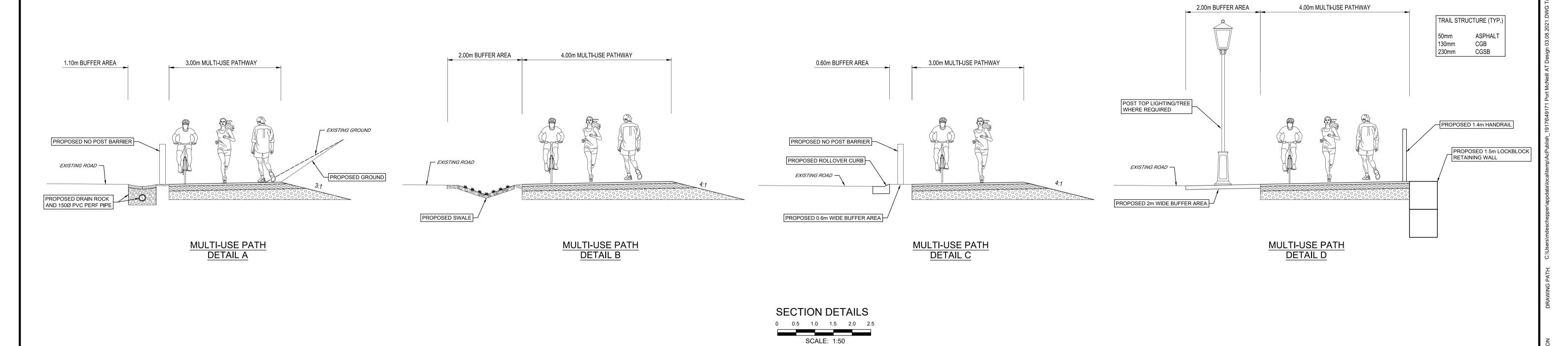
Project: 2221-49171-2019

Town of Port McNeill Class C Construction Cost Estimate Multi-use Pathway on Mine Rd

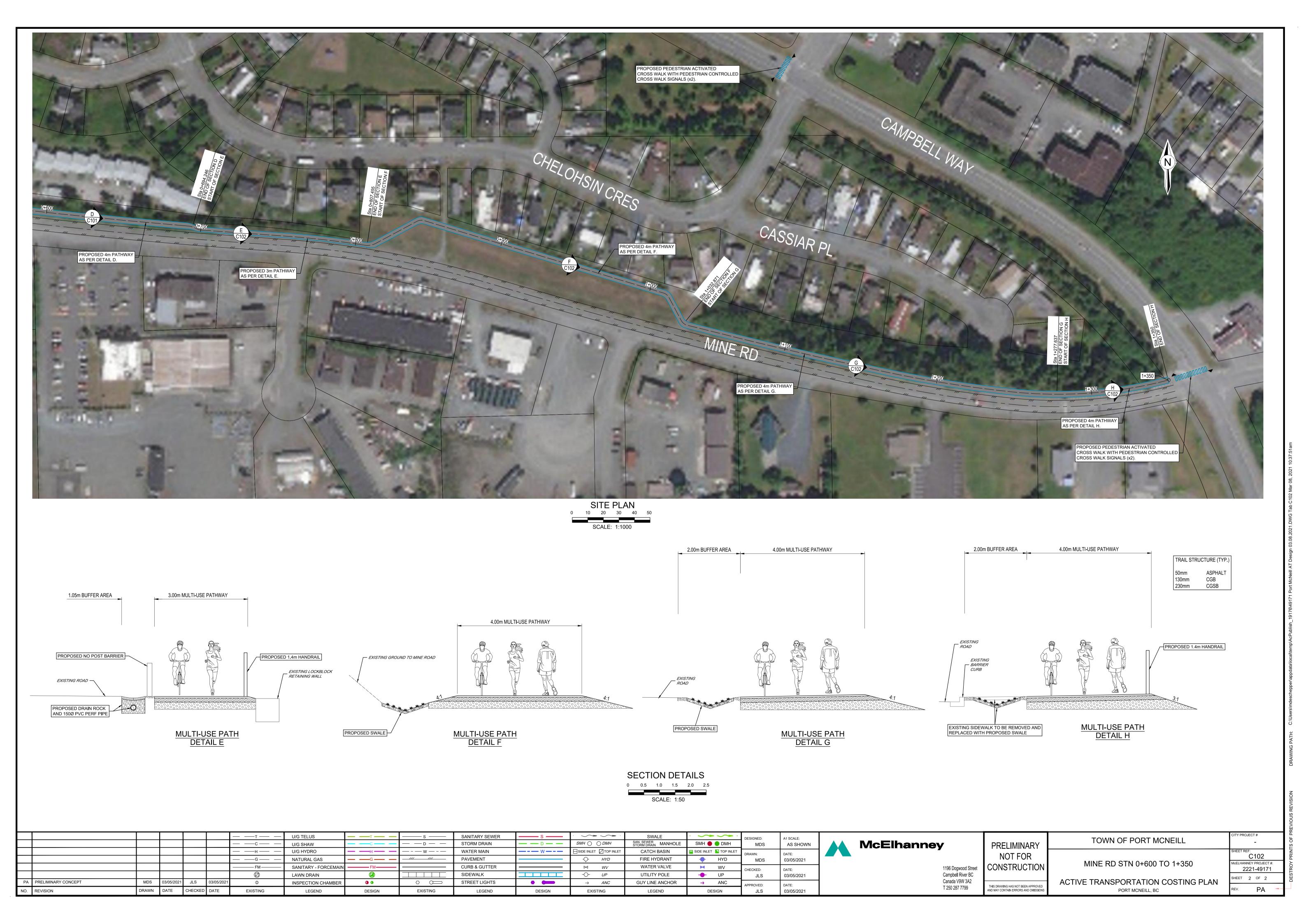
			Е	BUDGET		
	Estimated					
	Quantity	Unit	U	Init Cost	Est	imated Cost
General Requirements						
Mobilization & Demobilization, shall not exceed 10% of the Total Price, excluding GST	1	LS	\$	75,000	\$	75,0
Traffic Control, Vehicle Access and Parking	1	LS	\$	50,000	\$	50,0
Prepare for Approval a Construction Environmental Management Plan (CEMP)	1	LS	\$	5,000	\$	5,0
CEMP Implementation, Maintenance, Monitoring and Reporting Per Month	4	EA	\$	3,000	\$	12,0
Trail Signage and Markings	1	LS	\$	25,000	\$	25,0
Drainage Infrastructure Allowance	1	LS	\$	150,000	\$	150,0
Removals						
Common Excavation	3879	CM	\$	40	\$	155,1
Over Excavation c/w Import Backfill	560	CM	\$	80	\$	44,8
Import Pit Run	2552	CM	\$	30	\$	76,
BC Hydro Kiosk Relocation	1	LS	\$	20,000	\$	20,0
Clearing and Grubbing	1	LS	\$	100,000	\$	100,0
Multi-Use Pathway ROW						
Rollover Curb and Gutter	140	LM	\$	175	\$	24,
50mm Asphalt	5400	SM	\$	60	\$	324,
130mm Base Course (CGB)	6400	SM	\$	15	\$	96,
230mm Subbase Course (CGSB)	7000	SM	\$	12	\$	84,
Paint Markings for Crossing (Argus Dr)	1	EA	\$	2,000	\$	2,
Retaining Structure (1.5m High)	247.5	SM	\$	400	\$	99,
Infiltration Swale	400	LM	\$	100	\$	40,
No Post Barrier	260	LM	\$	75	\$	19,
Trail Features						
Lighting - 6m Post Top Fixtures	45	EA	\$	6,000	\$	270,
Lighting Wiring Ducting and Terminations	1350	LM	\$	45	\$	60,
Handrail (1.4m High)	339	LM	\$	170	\$	57,
Benches	7	EA	\$	1,000	\$	7,
Trees	90	EA	\$	750	\$	67,
				-Total	\$	1,630,
	General Contin					489,
Allowance for Engineering, Legal, Construction, Fina	incial and Admi	nistrati	on C	osts (15%)	\$	244,0
	Total (Rounde	ed to ne	ares	t \$1000)	\$	2,365,







				-	— т— —	U/G TELUS		s	SANITARY SEWER	s	. ~> ~> .	SWALE	. ~~	DESIGNED:	A1 SCALE:			TOWN OF DODT MONEY	CITY PROJECT #
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				-	— —н— —	U/G HYDRO		w	WATER MAIN	——— W———	☐SIDE INLET ☐TOP INLET	CATCH BASIN	SIDE INLET TOP INLET	DRAWN:	DATE:				SHEET REF:
				-	— —G— —	NATURAL GAS	— — G — —		PAVEMENT		-Q- HYD	FIRE HYDRANT	→ HYD	MDS	03/05/2021		NOT FOR	MINE DD 01000 TO 01600	McELHANNEY PROJE
				-	FM	SANITARY - FORCEMAIN	FM		CURB & GUTTER		⋈ WV	WATER VALVE	₩ WV	CHECKED:	DATE:	1196 Dogwood Stre	et CONSTRUCTION	MINE RD 0+000 TO 0+600	2221-49
						LAWN DRAIN			SIDEWALK		-⊖- UP	UTILITY POLE	→ UP	JLS	03/05/2021	Campbell River BC			SHEET 1 OF
PA F	PRELIMINARY CONCEPT	MDS	03/05/2021	JLS 03/05/2021	Φ	INSPECTION CHAMBER	() ()	O D	STREET LIGHTS	• •	\rightarrow ANC	GUY LINE ANCHOR	→ ANC	APPROVED:	DATE:	Canada V9W 3A2	THIS DRAWING HAS NOT BEEN ADDROVED	ACTIVE TRANSPORTATION COSTING PLAN	
IO. F	REVISION	DRAWN	DATE	CHECKED DATE	EXISTING	LEGEND	DESIGN	EXISTING	LEGEND	DESIGN	EXISTING	LEGEND	DESIGN	JLS	03/05/2021	T 250 287 7799	AND MAY CONTAIN ERRORS AND OMISSIONS	PORT MCNEILL, BC	REV. PA



Appendix 6
Grants

- BC Active Transportation Infrastructure Grant Program
- COVID-19 Resilience Infrastructure Stream
- Investing in Canada Infrastructure Program
- Community Economic Recovery Infrastructure Program
- Community Health Community program

BC Active Transportation Infrastructure Grant Program

Program Overview

The B.C. Active Transportation Infrastructure Grants Program provides cost-sharing opportunities for network planning grants and infrastructure grants. Funding from these grant programs support the development of active transportation infrastructure for all ages and abilities. For example, infrastructure grants fund:

- Multi-use protected travel lanes
- Pedestrian and cycling safety improvements
- End-of-trip facilities and other amenities
- Lighting and way-finding
- Successful recipients of a grant will:
- Be asked to sign a Conditional Grant Agreement
- · Receive initial funding when the agreement is signed
- Submit before and after photos
- Submit expense reports with supporting documentation
- Receive the remainder of the grant funding when the agreement is fulfilled

Eligibility

Eligible governments may apply for active transportation infrastructure funding if projects satisfy the following criteria:

- Previously funded active transportation projects (formerly BikeBC) awarded on or before 2018/2019 are complete by the time of the application submission
- Project is part of an active transportation network plan or equivalent
- Project can begin construction once provincial funding has been announced
- Projects will be completed by March 2021 (projects under \$1 million) or by March 2022 (projects over \$1 million)
- Projects are open to the public

Intake Status

The program has not been open to 2021/22 intake.

COVID-19 Resilience Infrastructure Stream

Program Overview

VRIS supports projects that:

- Provide retrofits, repairs and upgrades to local government and indigenous government buildings, health infrastructure and educational infrastructure
- Support development of active transportation networks
- Allow communities to improve their infrastructure to increase the resiliency and efficiency in preventing the spread of COVID-19 (including protective screens and ability to physically distance)
- Complete disaster mitigation and adaptation infrastructure projects

The applications that receive the highest technical score will be those that:

- Show a clear connection to program criteria and objectives
- Demonstrate the principles of sustainability
- And are supported by strong planning to ensure sustainable service delivery

Eligibility

Eligible projects will support public infrastructure, defined as a tangible capital asset primarily for public use and benefit. To be eligible, projects must meet at least one of the following outcomes specific to the CVRIS funding stream:

- Retrofits, repairs and upgrades to local government and indigenous buildings, health infrastructure or educational infrastructure
 - Local Government and Indigenous Government buildings
 - Health Infrastructure
 - Educational Infrastructure
- COVID-19 Resilience Infrastructure
 - Any public infrastructure asset¹ where the purpose of the project is to build, modify or reconfigure the asset to respond to the COVID-19 pandemic. This includes temporary infrastructure.
- Active Transportation Infrastructure
 - Active transportation infrastructure, including parks, trails, foot bridges, bike lanes, multi-use paths, parks and playgrounds

- Disaster mitigation and Adaptation Infrastructure²
 - Any public infrastructure asset¹, including natural infrastructure³, where the purpose of the project is to build, modify, or reinforce to prevent, mitigate or protect against the impacts of climate change, disasters triggered by natural hazards, or extreme weather events.²
 - Any public infrastructure asset¹ including natural infrastructure³, where the purpose of the project is to build, modify, or reinforce to prevent, mitigate or protect against fires.
 - Assets associated with afforestation and reforestation.

Intake Status

2021 intake closed January 27, 2021.

Investing in Canada Infrastructure Program – Northern and Rural Communities

Program Overview

The Rural and Northern Communities (RNC) Program will fund infrastructure projects that support the unique challenges faced by rural communities. Along with outcomes specific to the RNC Program, eligible outcomes under the Environmental Quality (EQ) and Community, Culture and Recreation (CCR), as well as the outcomes related to Public Transit and Adaptation, Resilience and Disaster Mitigation, will be eligible under the RNC Program. Allowing these alternative outcomes will enable smaller, rural communities to access funding for a variety of project types and receive a larger proportion of funding from senior governments.

Eligibility

In order to be eligible to apply for the RNC program, a project must meet one of the following core federal outcomes or one of the alternative federal outcomes from other streams listed below.

- Improved food security
- Improved and/or more reliable road, air and/or marine infrastructure
- Improved broadband connectivity
- More efficient and/or reliable energy Improved education and/or health facilities

An eligible rural community is defined as:

A local government established by or under British Columbia statute that is:

- An incorporated municipality that has a population of 25,000 persons or less.
- A regional district submitting an application for a community that has a population of 25,000 persons or less. A community, for the purpose of application to the program, is considered to be a settlement area within a regional district electoral area. A community's boundaries may also coincide with a service area boundary (existing or proposed).

Intake Status

2020 intake closed October 2020.

Community Health Community program

Program Overview

The Healthy Communities Initiative supports communities as they create and adapt public spaces, and programming and services for public spaces to respond to ongoing needs arising from COVID-19 over the next two years. This \$31 million investment from the Government of Canada will fund small-scale infrastructure projects to create safer, more vibrant and inclusive communities. Community Foundations of Canada (CFC) and its network are working alongside the Canadian Urban Institute (CUI) and other partners to deliver the Healthy Communities Initiative locally.

Eligibility

Healthy Communities Initiative projects will: ● respond to identified needs arising from impacts of COVID-19; ● create and adapt public spaces, and programming and services for public spaces in the public interest; ● demonstrate consideration of and connections with the community; ● serve the general public or a community disproportionately impacted by COVID-19; and, ● fall within the three Healthy Communities Initiative theme areas.

Eligible organizations include: • Municipalities and local or regional governments • Municipally-owned corporations (e.g. water management, public transit, economic development, tourism agencies, etc.) Indigenous governing bodies including, but not limited to: a. A band council within the meaning of section 2 of the Indian Act; b. A First Nation, Inuit or Métis government or authority established under a Self-Government Agreement or a Comprehensive Land Claim Agreement; c. A First Nation, Inuit or Métis

government that are established by or under legislation whether federal or provincial or territorial that incorporates a governance structure; d. Tribal councils, provincial/territorial bodies

Intake Status

There will be two application rounds of funding applications. Applicants who did not receive funding in the first round can reapply for funding in round two but will not be guaranteed funding. Round one The application portal will open on February 9, 2021, at 9:00 AM AST. Applications must be submitted by March 9, 2021, at 5:00 PM PST. Review committees will start meeting to make decisions from March 10, 2021 onwards and all applicants will receive results by April 30, 2021. 13 Openness Being clear about what data will be attributed to individuals and how any data might be made available for sharing in open-data formats and/or as files to download such as an Excel sheet. Compliance Aligns with municipal, provincial or federal requirements such as Personal Information Protection and Electronic Documents Act (PIPEDA). Round two The application portal will open May 14, 2021, at 9:00 AM AST. Applications must be submitted by June 25, 2021, at 5:00 PM PST. Applicants who did not receive funding in round one may re-apply to round two but will not be guaranteed funding. Review committees will start meeting on June 26, 2021 and all applicants will receive results by August 13, 2021, at the latest.

Community Economic Recovery Infrastructure Program

Program Overview

Under the Community Economic Recovery Infrastructure Program (CERIP), the Province is committing up to \$90 million to support community economic resilience, tourism, heritage, and urban and rural economic development projects.

The projects will help communities impacted by COVID-19 and support B.C.'s post-pandemic economic recovery.

Eligible applicants can receive a one-time, 100% funded provincial grant to support four key streams:

- Community Economic Resilience
- Destination Development
- Unique Heritage Infrastructure
- Rural Economic Recovery

Eligibility

To be eligible for funding, a project must:

- Be for public use and benefit, and be for construction, development, economic investment, or cultural heritage works
- Start construction in 2021
- Meet all applicable impact assessment, provincial health or environmental legislation and standards
- Meet specific outcomes of the selected project funding stream:
 - o Community Economic Resilience
 - o Destination Development
 - o Unique Heritage Infrastructure
 - o Rural Economic Recovery

Intake Status

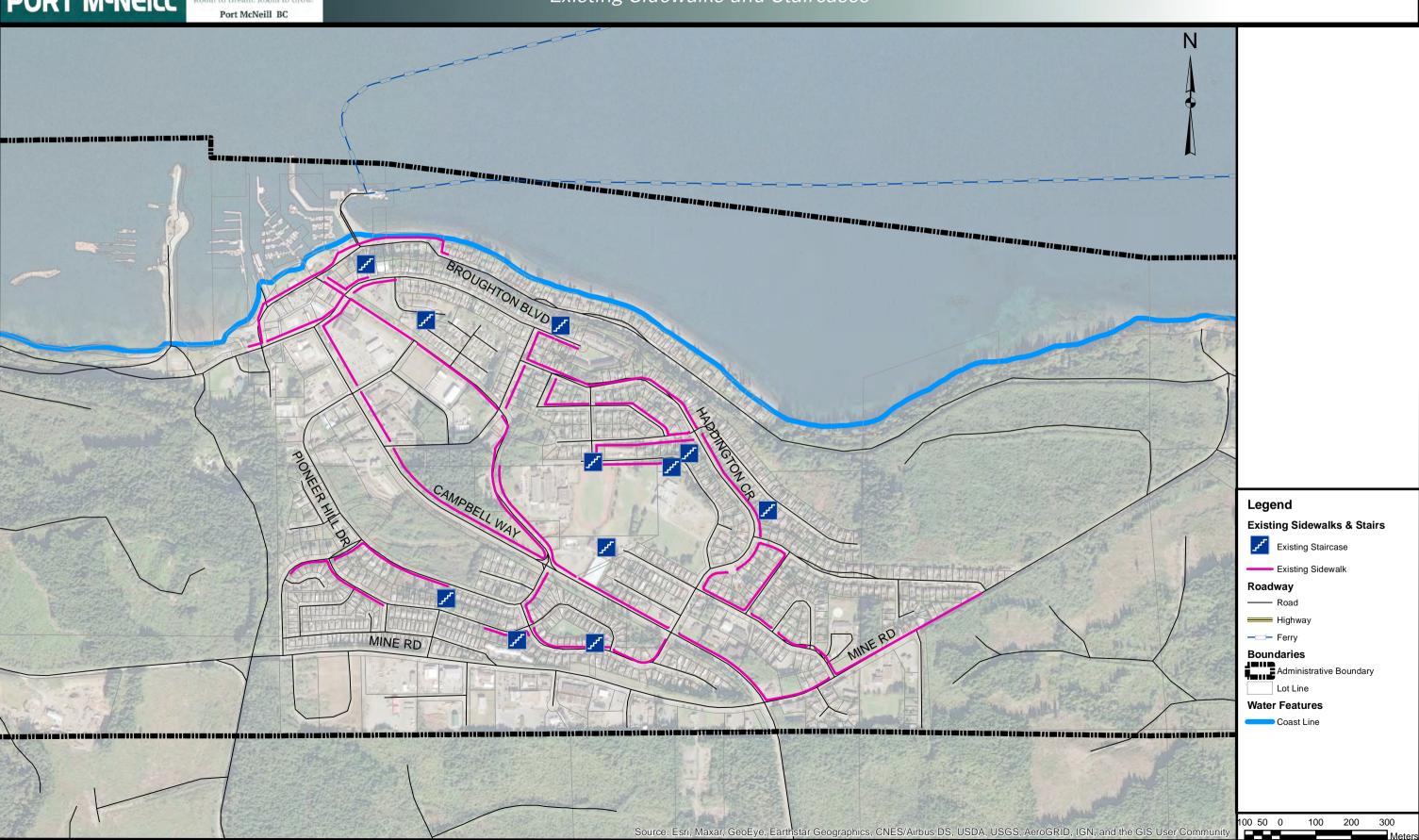
Closed for 2021 intake.

Appendix 7
Maps



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Existing Sidewalks and Staircases

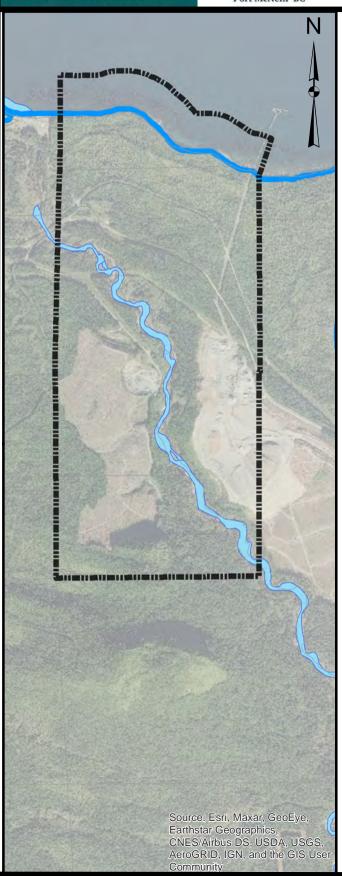


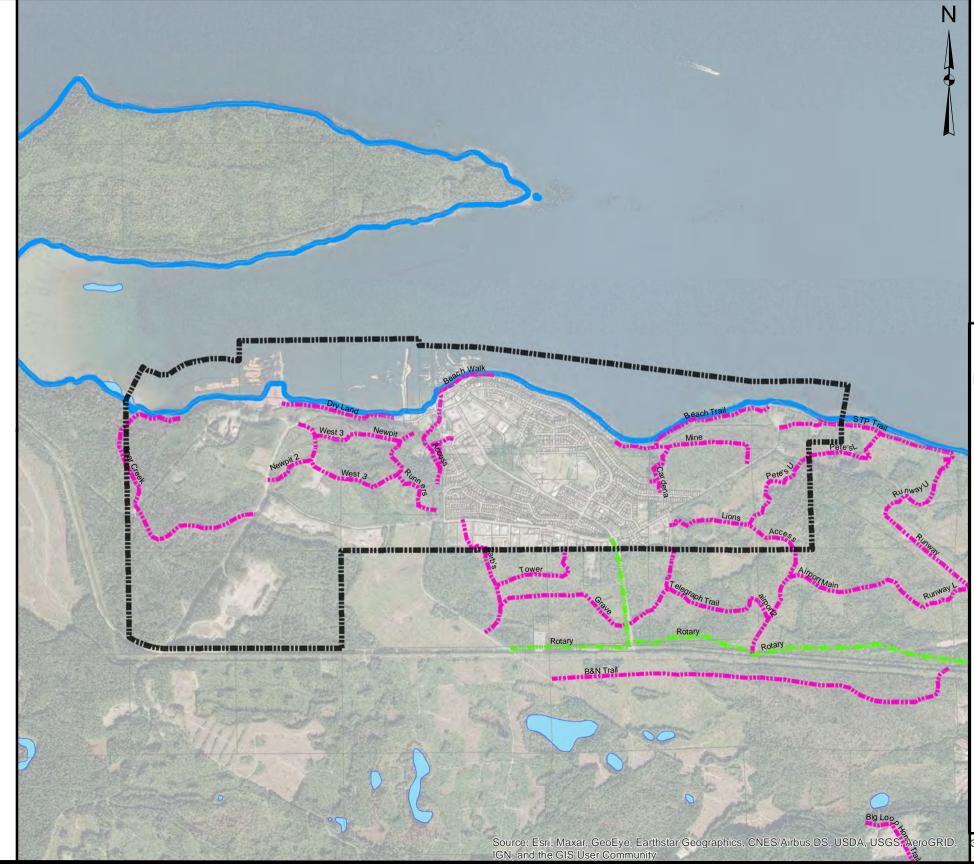




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Location of Trails





EXISTING TRAIL AND PATH DATA

- 1) Trails highlighted in magenta have been provided by Strategic Natural Resource Consultants Inc. on 2021-
- 2) Trails highlighted in cyan have been provided by on 2021-02-.

Legend

Boundaries

Administrative Boundary

Lot Line

Existing Trails

Informal Trail

Formal Trail

Water Features

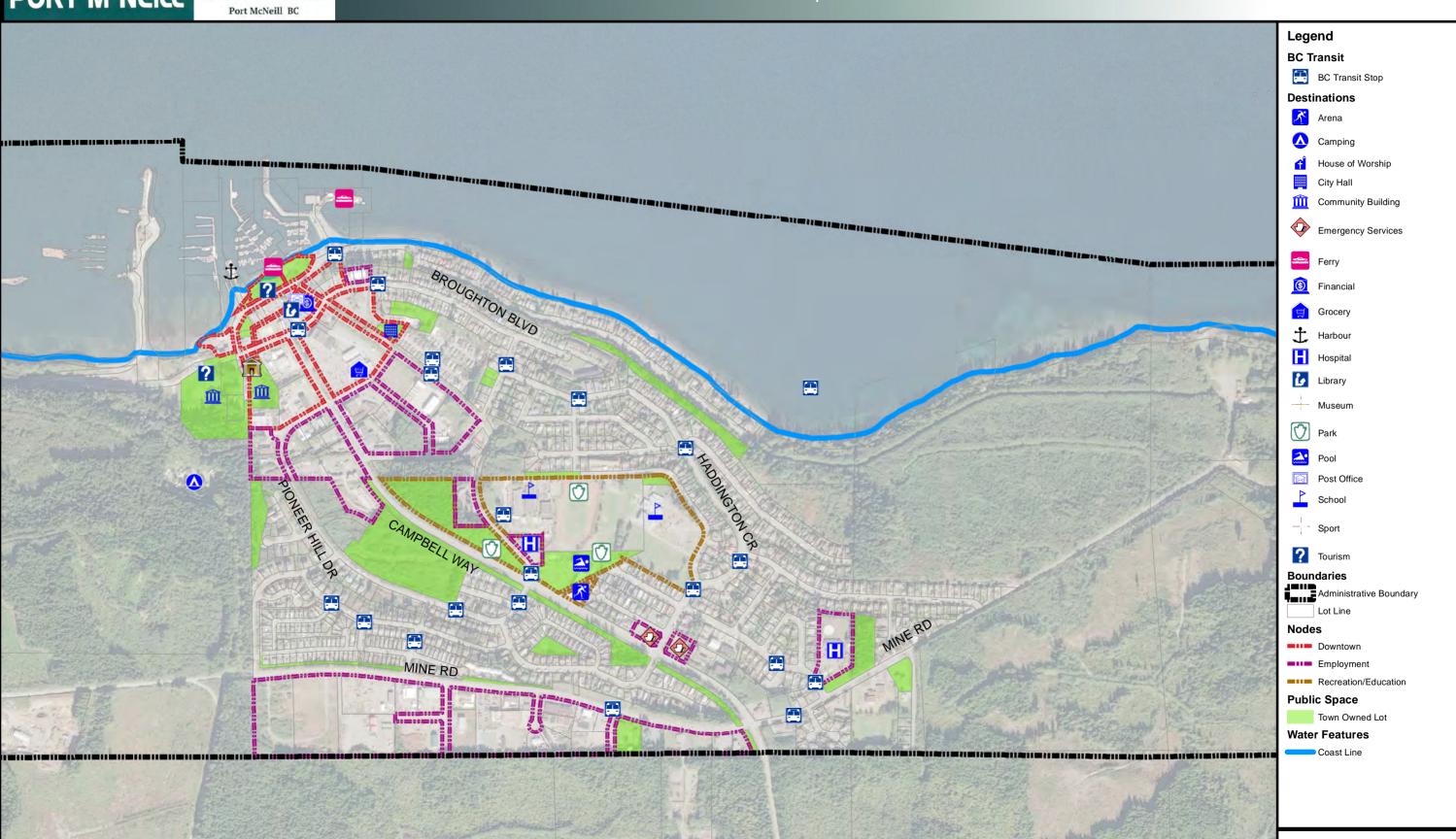
Coast Line

Streams, Rivers & Lakes



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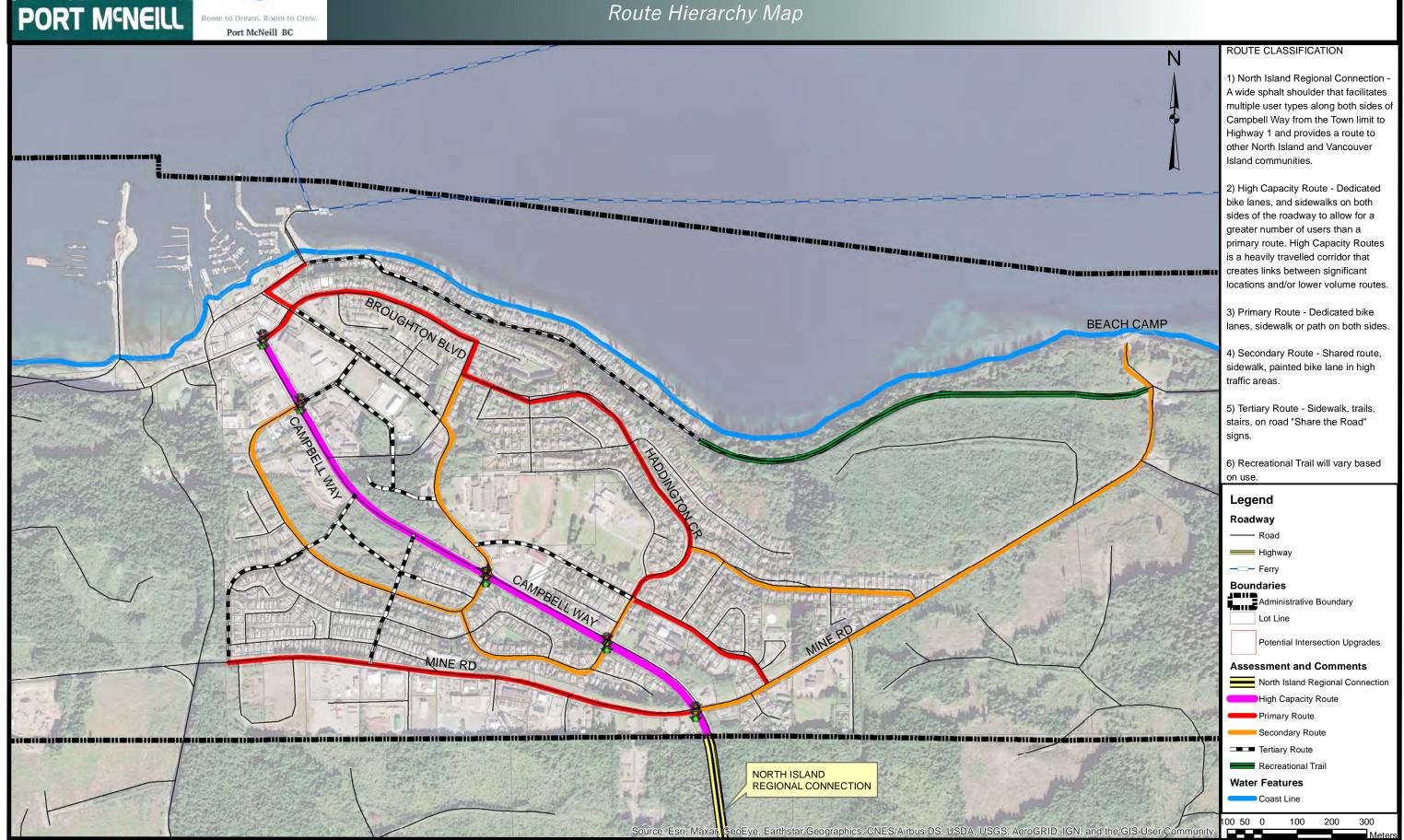
Destinations Map



Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

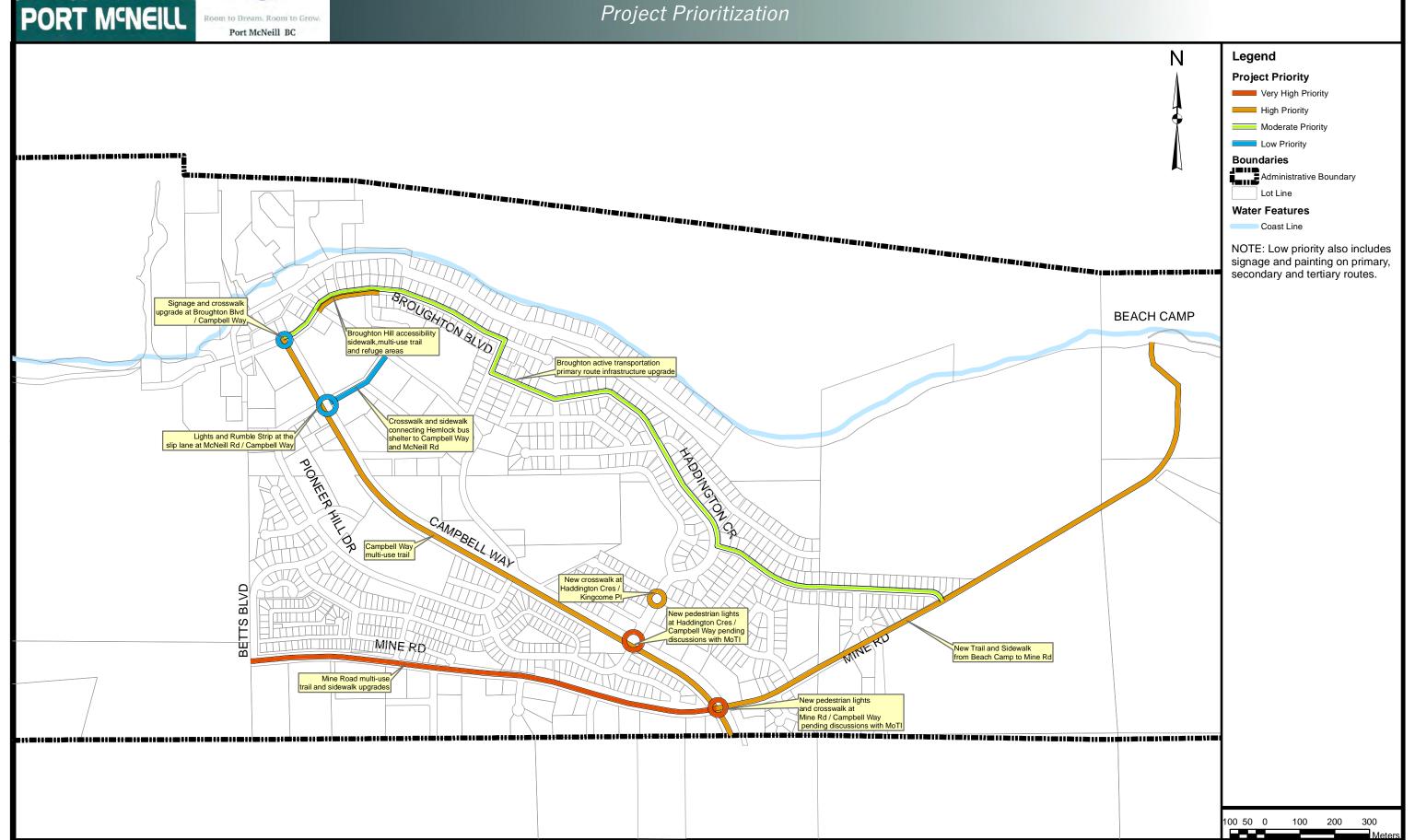


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Appendix 8

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Room to Dream. Room to Grow.

Port McNeill BC

